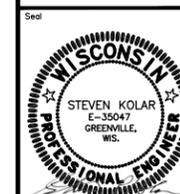


EXHIBIT 11

Capitol Drive Improvement Plans & Specifications

PLAN OF PROPOSED IMPROVEMENT FOR W. CAPITOL DRIVE (S.T.H. 83 TO EAST OF VETTELSON ROAD) VILLAGE OF HARTLAND, WISCONSIN WAUKESHA COUNTY



SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	GENERAL NOTES
3 - 4	TYPICAL SECTIONS
5	EROSION CONTROL PLAN
6 - 8	REMOVAL PLAN
9 - 11	ALIGNMENT PLAN
12 - 16	ROADWAY & STORM PLAN AND PROFILE
17 - 19	PAVING PLAN
20 - 22	PAVEMENT MARKING AND SIGNAGE
23 - 25	SIDEWALK RAMP DETAILS
26 - 31	CONSTRUCTION DETAILS
32 - 36	TRAFFIC CONTROL PLANS
37 - 62	CROSS SECTIONS



LOCATION MAP
1" = 500'

HORIZONTAL CONTROL IS BASED UPON WISCONSIN COUNTY
COORDINATE SYSTEM WAUKESHA COUNTY NAD 83 (2001).
VERTICAL CONTROL IS BASED UPON NAVD88.

PROJECT
W CAPITOL DRIVE
VILLAGE OF HARTLAND
WAUKESHA COUNTY, WI 53029

CLIENT
KWIK TRIP
1813 KRAMER ST
LA CROSSE, WI 54603

SHEET TITLE
TITLE SHEET

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WE-UTILITY-RELOCATIONS@WE-ENERGIES.COM

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ABBREVIATIONS

D	DEGREE OF CURVE
Δ	DELTA
EL	ELEVATION
G	GAS
INV	INVERT
L	LENGTH OF CURVE
NC	NORMAL CROWN
NTS	NOT TO SCALE
OH	OVERHEAD UTILITY
PC	POINT OF CURVE
PT	POINT OF TANGENT
R	RADIUS OF CURVE
R/L	REFERENCE LINE
R/W	RIGHT OF WAY
SAN	SANITARY
SDD	STANDARD DETAIL DRAWING
SE	SUPER ELEVATION
SS	STORM SEWER
SSPRC	STORM SEWER PIPE
RC	REINFORCED CONCRETE
STA	STATION
T	TANGENT LENGTH
TYP	TYPICAL
VCL	VERTICAL CURVE LENGTH
VPC	POINT OF VERTICAL CURVE
VPI	POINT OF VERTICAL INTERSECTION
VPT	POINT OF VERTICAL TANGENT
W	WATER

GENERAL NOTES

ALL RADII DIMENSIONS ARE MEASURED TO THE FACE OF CURB UNLESS NOTED OTHERWISE.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS, AS SHOWN ON THE PLANS, ARE APPROXIMATE. THERE MAY BE OTHER UTILITIES AND UTILITY INSTALLATIONS WITHIN THE PROJECT LIMITS THAT ARE NOT SHOWN.

NOTIFY DIGGERS HOTLINE AND AFFECTED UTILITIES PRIOR TO THE START OF WORK. ANY LOCAL MUNICIPAL UTILITY WHICH ARE NOT A MEMBER OF THE DIGGERS HOTLINE MUST BE CONTACTED SEPARATELY.

NEW HMA PAVMENT SHALL BE CONSTRUCTED IN TWO LIFTS CONSISTING OF A 2-INCH SURFACE (5MT 58-28H) AND 4-INCH BINDER (3MT 58-28S) LIFT.

TACK COAT SHALL BE APPLIED AT A RATE OF 0.07 GAL/SY.

EROSION CONTROL DEVICES ARE INCIDENTAL TO THE COST OF THEIR RESPECTIVE BID ITEMS.

INSTALL EROSION CONTROL MEASURES PRIOR TO START OF WORK OPERATIONS.

CURB AND GUTTER GRADES ARE TO FLANGE UNLESS NOTED OTHERWISE.

ALL DISTURBED AREAS SHALL BE FINISHED WITH TOPSOIL OR SALVAGED TOPSOIL, FERTILIZED, SEEDED, AND MULCHED UNLESS NOTED OTHERWISE.

RESTORATION OF EXPOSED SLOPES AND DITCHES SHALL TAKE PLACE NOT MORE THAN 7 CALENDAR DAYS AFTER GRADING IS COMPLETE UNLESS OTHERWISE DIRECTED BY THE VILLAGE.

TRAFFIC CONTROL DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS OR AS DIRECTED BY THE VILLAGE.

BENCHMARK LOCATIONS SHOWN ON PLAN SHALL BE VERIFIED PRIOR TO THE START OF CONSTRUCTION.

ALL HOLES OR OPENINGS BELOW SUBGRADE RESULTING FROM ABANDONMENT OR REMOVAL OF EXISTING STRUCTURES WITHIN RIGHT OF WAY SHALL BE FILLED WITH SLURRY BACKFILL AND OR AS DIRECTED BY THE VILLAGE.

ALL WORK SHALL CONFORM TO VILLAGE OF HARTLAND SPECIFICATIONS AND THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS, MOST RECENT EDITION.

RECTANGULAR RAPID-FLASHING BEACONS (RRFB'S) SHALL BE INSTALLED PER FHWA POLICY MEMORANDUM INTERIM APPROVAL 21 DATED MARCH 20,2018

UTILITY INSTALLATION
STORM SEWER, SANITARY SEWER AND WATER SERVICE WORK SHALL CONFORM TO THE STANDARD SPECIFICATIONS FOR SEWER AND WATER CONSTRUCTION IN WISCONSIN LATEST EDITION AND ADDENDUMS.

SLURRY BACKFILL IS REQUIRED IN THE EXISTING ROADWAY TO 5-FEET OUTSIDE PAVEMENT/CURB/SIDEWALK FOR ALL SANITARY SEWER/WATER MAIN WORK. STORM SEWER BACKFILL CAN BE HAULED IN COMPACTED GRANULAR MATERIAL (LIMESTONE SCREENINGS ARE NOT ALLOWED). BACKFILL AROUND ALL STRUCTURES ARE TO BE FLUSH CONSOLIDATED. 3-FT MAX GRID SPACING LATERALLY AND LONGITUDINALLY FOR FLUSHING TO A MINIMUM RADIUS OF 6-FT FROM THE STRUCTURES. MECHANICAL COMPACTION ACCEPTABLE ON MAINLINE STORM SEWER.

EXISTING SANITARY AND STORM STRUCTURES SHALL BE ADJUSTED TO GRADE WITH CONCRETE ADJUSTMENT RINGS. IF EXISTING CHIMNEY RINGS ARE CRACKED/BROKEN/IN NEED OF REPAIR - THEY SHOULD BE REPAIRED TO A DEPTH NEEDED TO GET RID OF ALL CRACKED/BROKEN EXISTING RINGS AND BROUGHT BACK TO SURFACE WITH NEW CONCRETE ADJUSTING RINGS.

TRACER WIRE OR OTHER APPROVED MEANS OF LOCATING UNDERGROUND NON METALLIC SEWERS/MAINS MUST BE PROVIDED IN ACCORDANCE WITH THE PROVISIONS IN THE WISCONSIN STATUTES 182.0175(2R) AND THE WISCONSIN DEPARTMENT OF SAFETY AND PROFESSIONAL SERVICES SPS 82.30(11)(H).

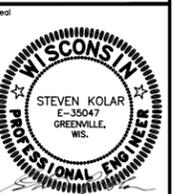
POTHOLE TO VERIFY THE POSITIONS OF EXISTING UNDERGROUND FACILITIES AT SUFFICIENT NUMBER OF LOCATIONS IN ORDER TO ASSURE THAT NO CONFLICT WITH THE PROPOSED WORK EXISTS AND THAT SUFFICIENT CLEARANCE IS AVAILABLE.

DESIGN CONTACT
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SKOLAR@WALBECGROUP.COM

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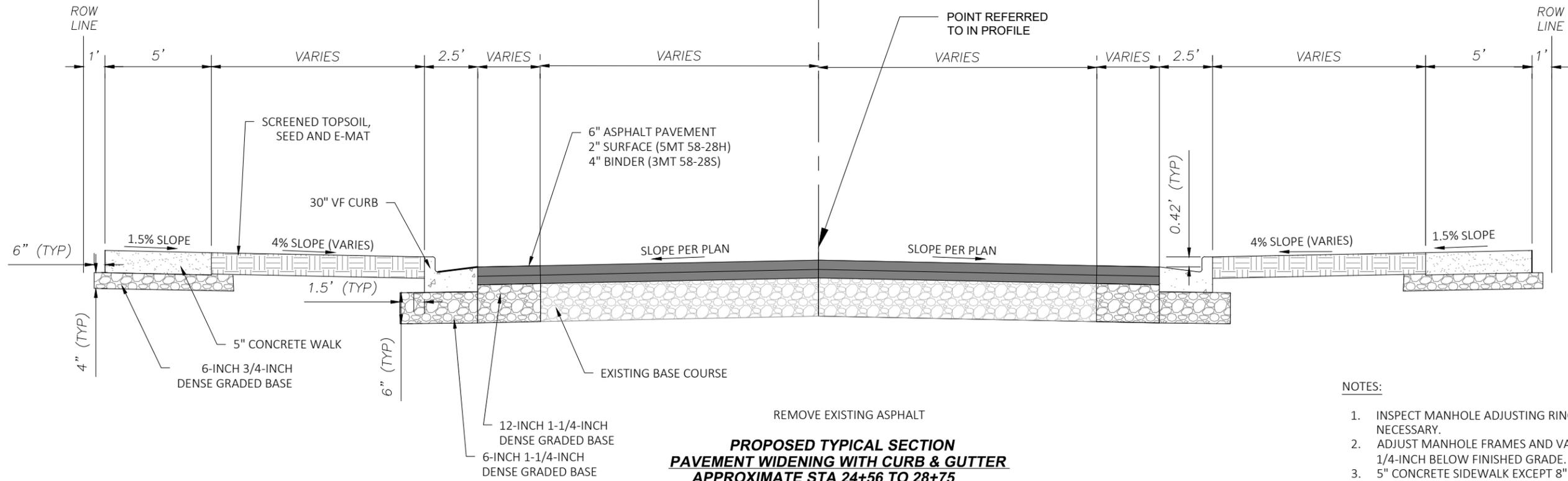
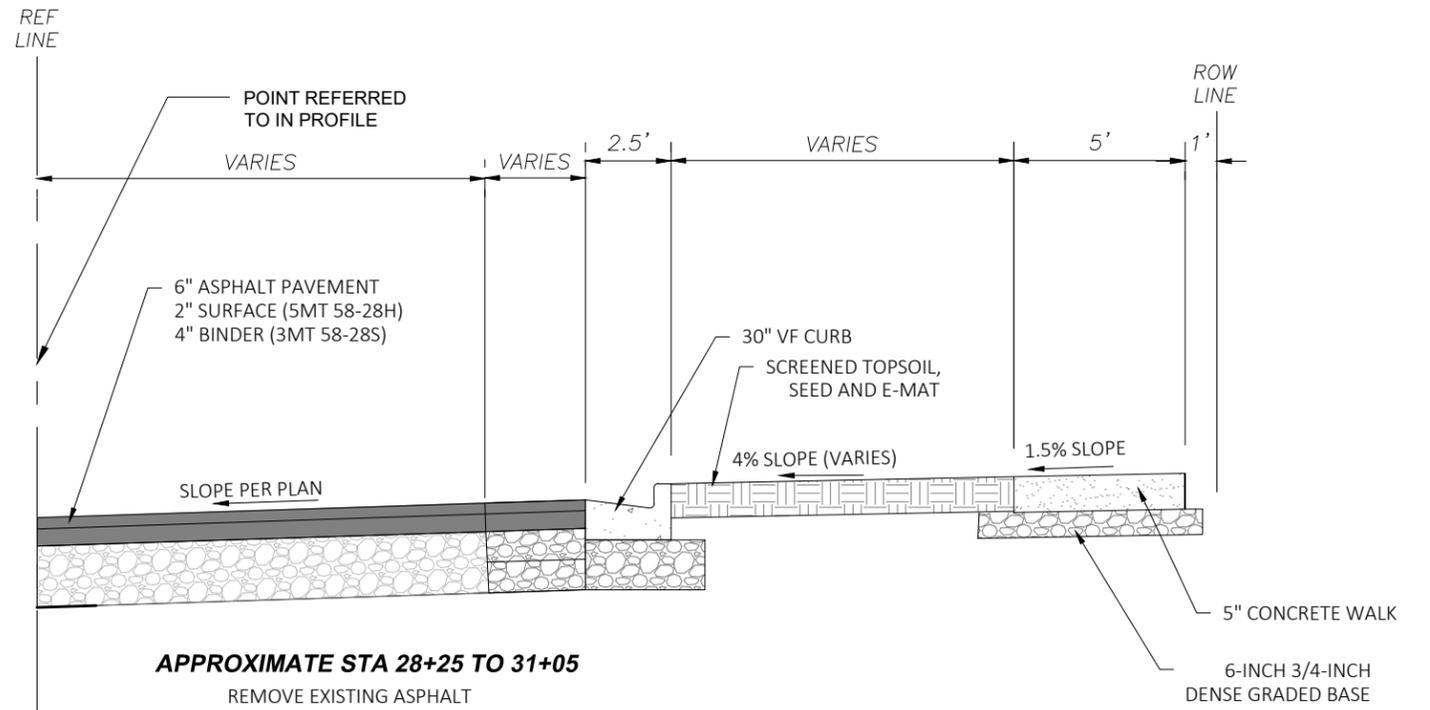


PROJECT: **W CAPITOL DRIVE**
VILLAGE OF HARTLAND
HARTLAND, WI 53029
CLIENT: **KWIK TRIP**
1813 KRAMER ST
LA CROSSE, WI 54603

GENERAL NOTES

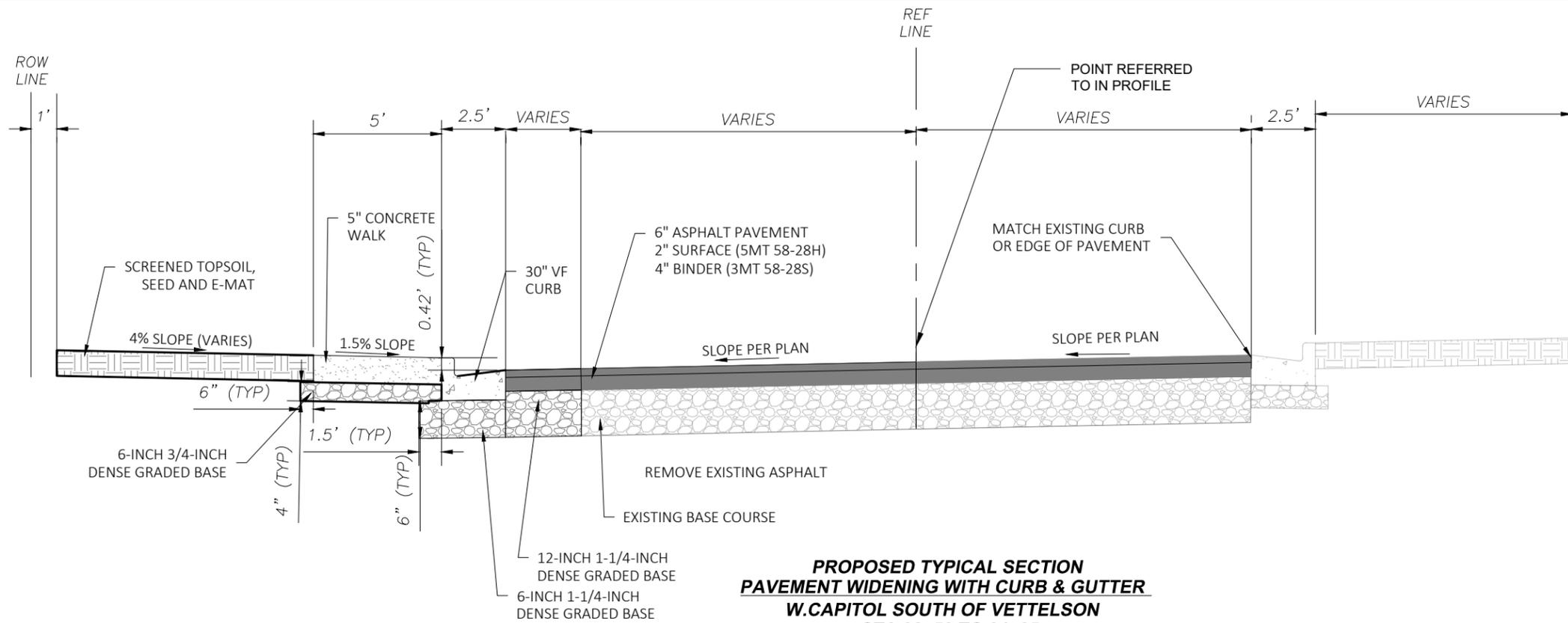
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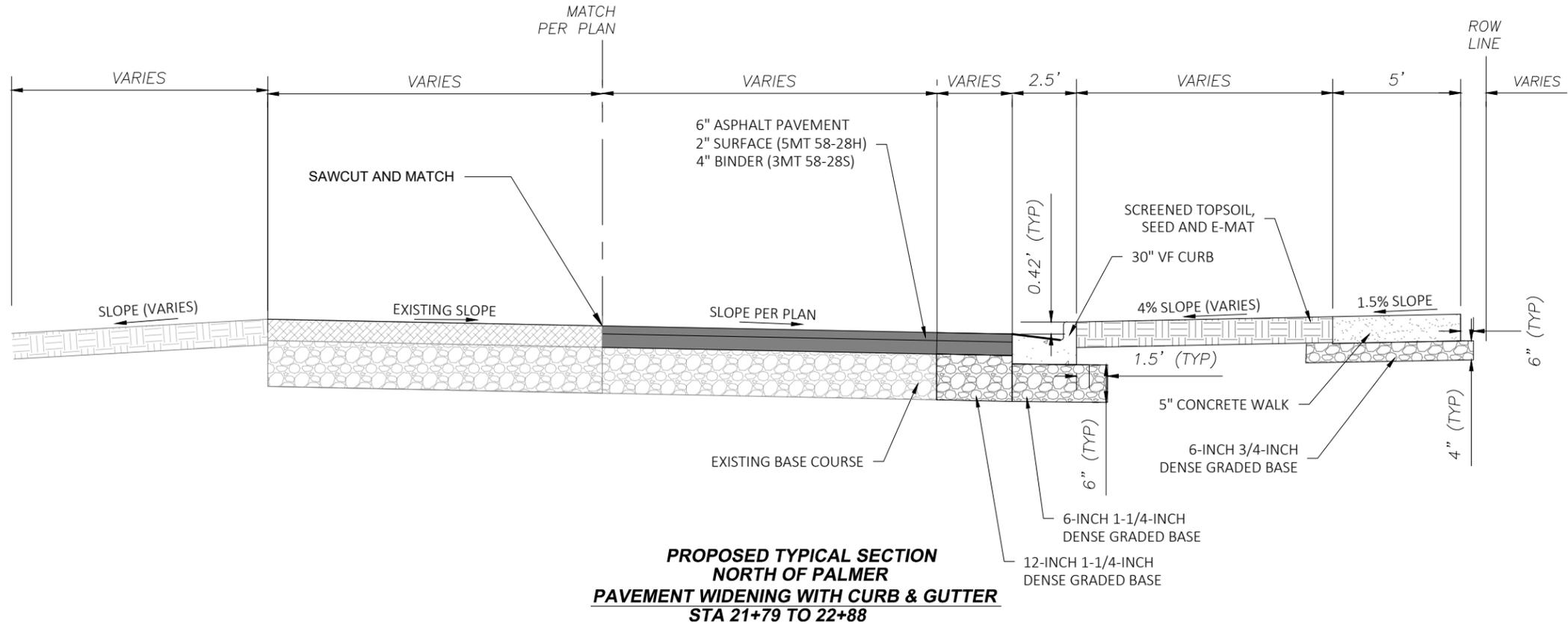
NOTES:

1. INSPECT MANHOLE ADJUSTING RINGS AND REPLACE IF NECESSARY.
2. ADJUST MANHOLE FRAMES AND VALVE BOXES TO 1/4-INCH BELOW FINISHED GRADE.
3. 5" CONCRETE SIDEWALK EXCEPT 8" THICK THROUGH DRIVEWAY APPROACHES (IF APPLICABLE).
4. COMPACT BASE TO 95% OF STANDARD PROCTOR.
5. DOES NOT INCLUDE INTERSECTIONS OR CROWN TRANSITIONS.



- NOTES:**
1. INSPECT MANHOLE ADJUSTING RINGS AND REPLACE IF NECESSARY.
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 4. COMPACT BASE TO 95% OF STANDARD PROCTOR.
 5. DOES NOT INCLUDE INTERSECTIONS OR CROWN TRANSITIONS.

**PROPOSED TYPICAL SECTION
PAVEMENT WIDENING WITH CURB & GUTTER
W.CAPITOL SOUTH OF VETTELSON
STA 32+50 TO 34+35**
N.T.S.

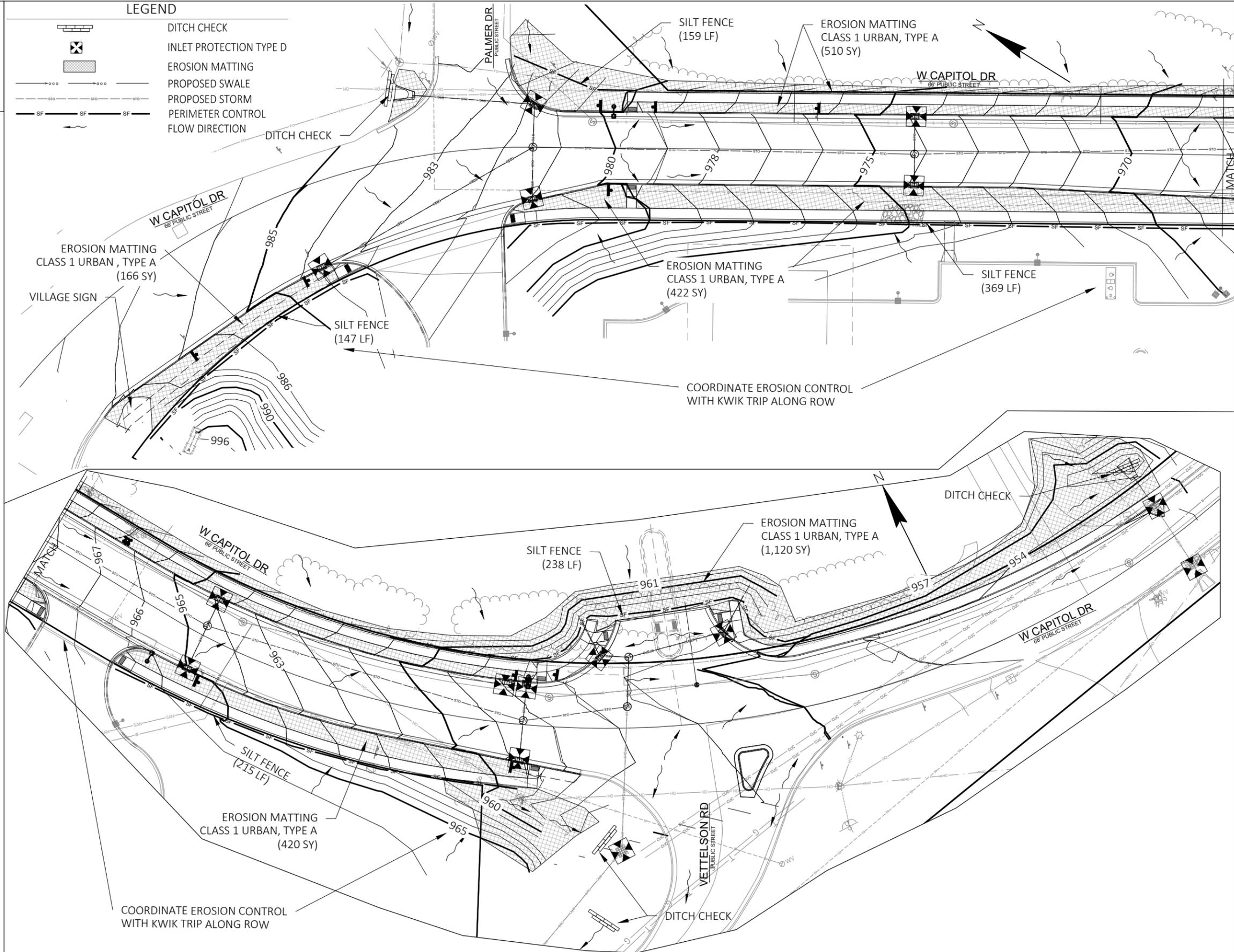


**PROPOSED TYPICAL SECTION
NORTH OF PALMER
PAVEMENT WIDENING WITH CURB & GUTTER
STA 21+79 TO 22+88**
N.T.S.

PROJECT NO: 490823	HWY: W. CAPITOL DRIVE	COUNTY: WAUKESHA	TYPICAL SECTIONS	SHEET 4	E
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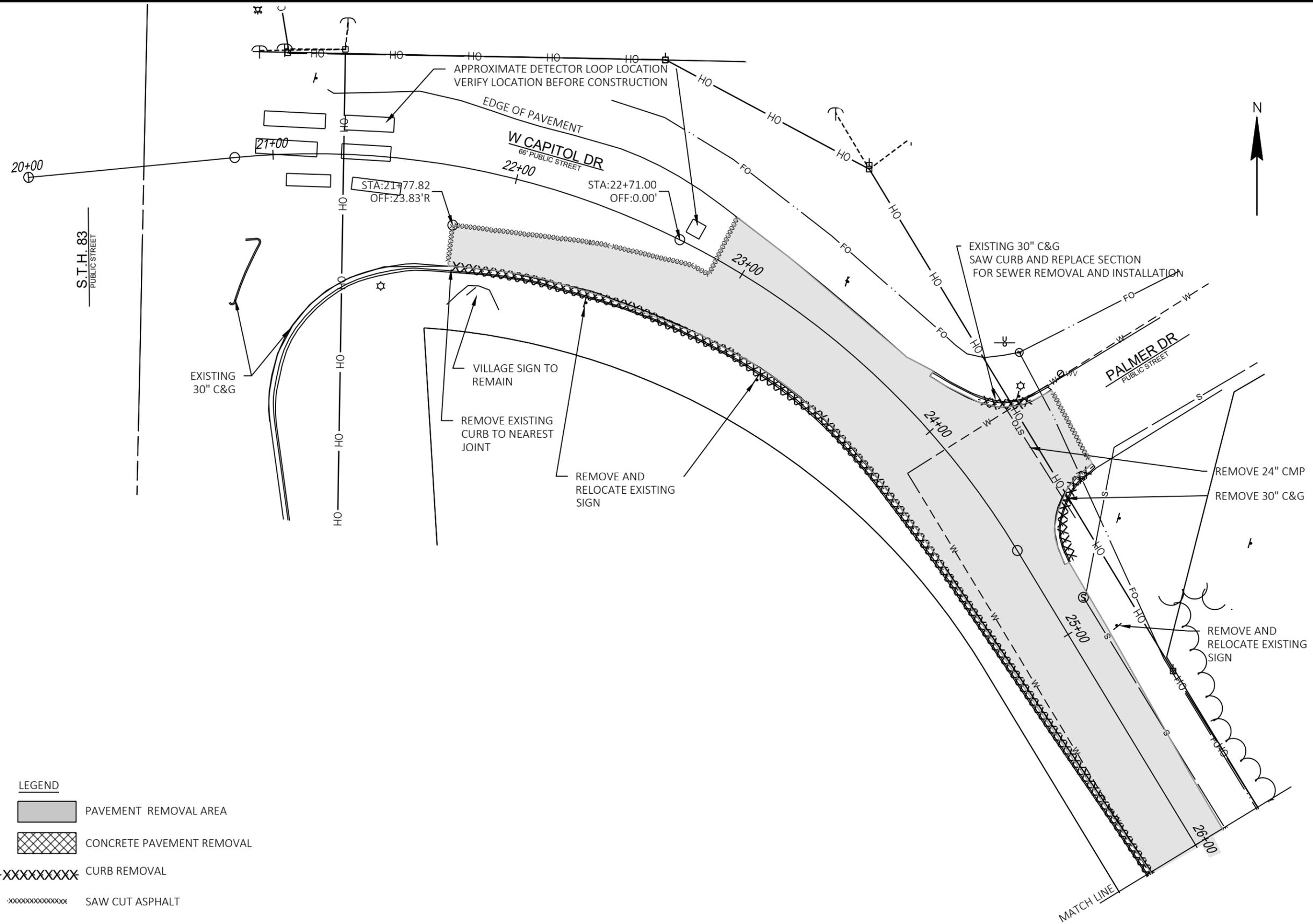
LEGEND

-  DITCH CHECK
-  INLET PROTECTION TYPE D
-  EROSION MATTING
-  PROPOSED SWALE
-  PROPOSED STORM
-  PERIMETER CONTROL
-  FLOW DIRECTION

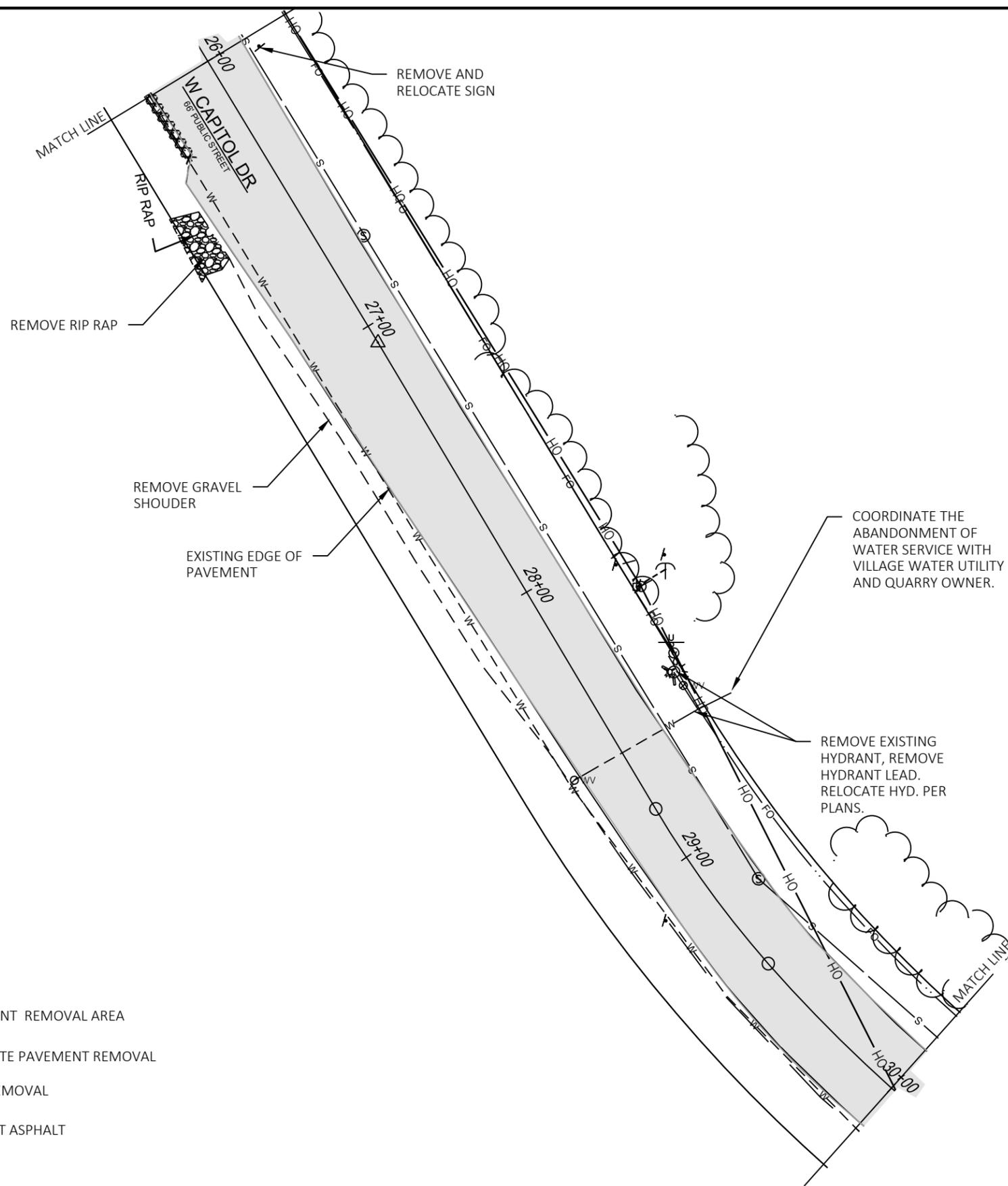


MAINTENANCE PLAN

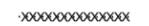
1. ALL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE INSPECTED FOR FUNCTIONAL EFFECTIVENESS FOLLOWING EVERY 1/2-INCH RAIN EVENT AND IN NO CASE LESS THAN ONCE PER WEEK. ANY NECESSARY REPAIRS WILL BE MADE IMMEDIATELY TO ENSURE THE DEVICE FUNCTIONS AS DESIGNED.
2. ALL SEEDED AREAS WILL BE WATERED, FERTILIZED, MULCHED AND RE-SEEDED AS NECESSARY TO MAINTAIN A VIGOROUS, DENSE VEGETATIVE COVER.
3. ANY SEDIMENT REACHING A PUBLIC OR PRIVATE STREET SHALL BE REMOVED BY STREET CLEANING BEFORE THE END OF EACH DAY OR AS NECESSARY TO PREVENT DEBRIS FROM ENTERING A STORM SEWER, DITCH OR WATERWAY.
4. IN THE EVENT CONSTRUCTION ACTIVITIES RESULT IN SEVERE DUST CONDITIONS, SEE WDNR TECHNICAL STANDARD 1068.
5. IN THE EVENT OF DEWATERING ACTIVITIES, FOLLOW WDNR TECHNICAL STANDARD 1061. ALL WATER DISCHARGED FROM THE DEWATERING EQUIPMENT MUST BE FILTERED BY A TYPE II GEOTEXTILE BAG SIZED FOR THE PUMP DISCHARGE FLOW RATE.
6. SEDIMENT TRAP DESIGN TO BE IN ACCORDANCE WITH WDNR TECHNICAL STANDARD 1063.
7. DITCH CHECKS TO BE PLACED AT EVERY 2' OF VERTICAL DROP IN ELEVATION OR CLOSER. DITCH CHECK AND EMAT INSTALLATION SHOULD BE PER WDNR TECHNICAL STANDARD 1062.
8. DRAINAGE WAYS ARE TO BE SEEDED AND MULCHED OR SODDED AS SOON AS POSSIBLE. SIDE SLOPES OF DRAINAGE WAYS THAT ARE SLOPED 4:1 OR GREATER AND FLOW LINES OF THE DRAINAGE WAYS SHALL HAVE EROSION FABRIC OR MATTING INSTALLED TO PROTECT FROM EROSION. LIGHT DUTY WISDOT CLASS I URBAN TYPE A MAT IS RECOMMENDED FOR THE SLOPED AREAS AND TYPE B CLASS II FOR THE FLOW LINE OF THE SWALES.
9. ALL EXPOSED SOIL AREAS NOT DISTURBED FOR UP TO SEVEN DAYS MUST BE IMMEDIATELY STABILIZED OR PERMANENTLY RESORED IN ACCORDANCE WITH THE WDNR BMP'S.
10. TEMPORARY STABILIZATION MATERIALS SHALL BE MULCHED AND SHALL BE APPLIED AND ANCHORED AS SPECIFIED IN WDNR TECHNICAL STANDARD 1058. FOR AREAS NOT STABILIZED BY OCT. 15. APPLY TEMPORARY SEEDING WITH ANIONIC POLYACRYLAMIDE AS SPECIFIED IN WDNR TECHNICAL STANDARD 1050 & 1059.
11. ALL SIGNIFICANT CHANGES TO THIS SCHEDULE SHALL BE PROVIDED TO THE WDNR AND LOCAL MUNICIPALITY PRIOR TO DEVIATION.
12. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN POSITIVE DRAINAGE TO THE SEDIMENT CONTROL DEVICES AT ALL TIMES DURING CONSTRUCTION.
13. ALL EROSION CONTROL BMP DEVICES CAN BE REMOVED ONCE 100% OF THE SITE HAS BEEN VEGETATED TO 70% COVERAGE PER WDNR TECHNICAL STANDARDS.

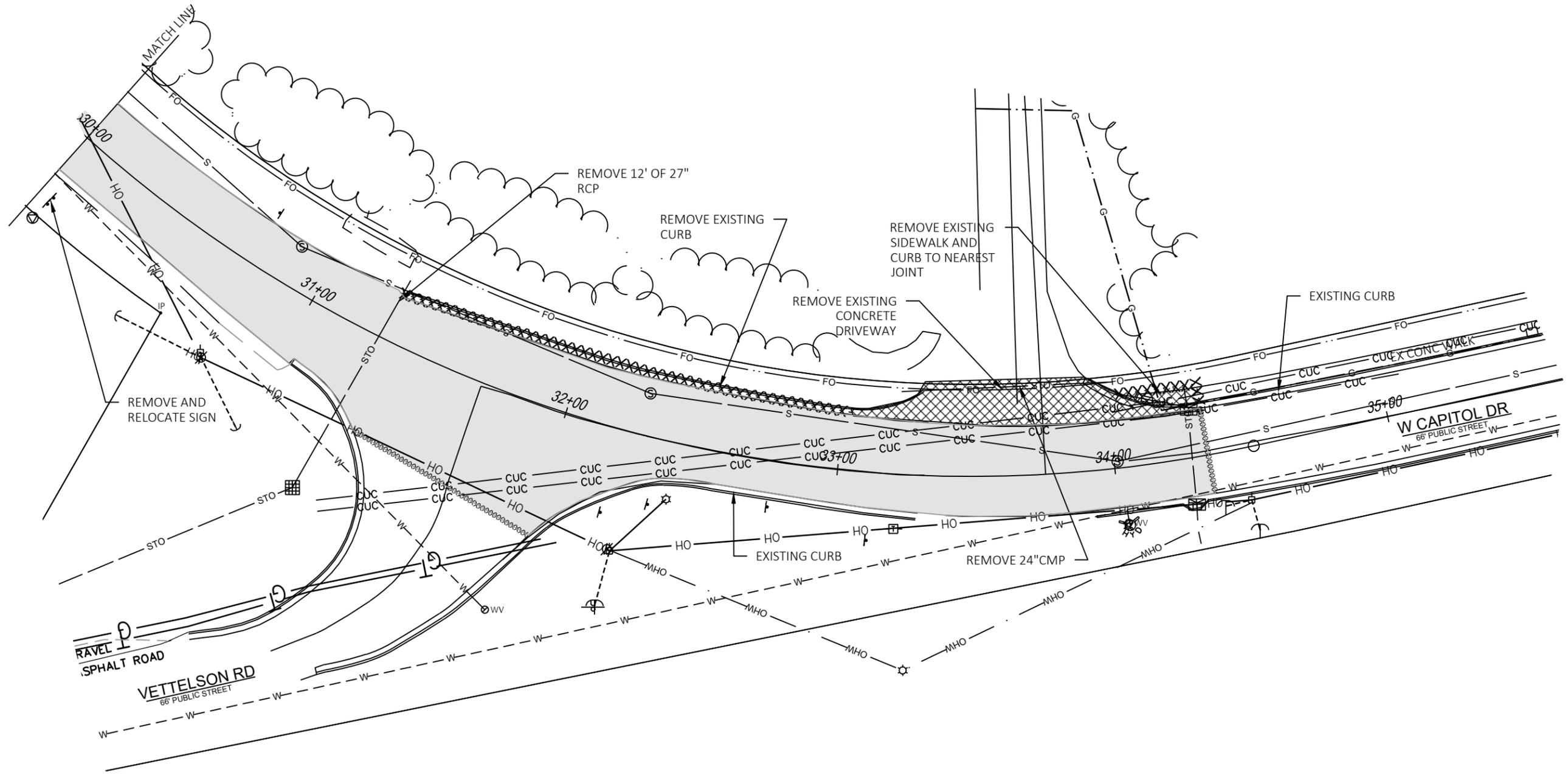


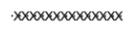
- LEGEND**
- PAVEMENT REMOVAL AREA
 - CONCRETE PAVEMENT REMOVAL
 - CURB REMOVAL
 - SAW CUT ASPHALT

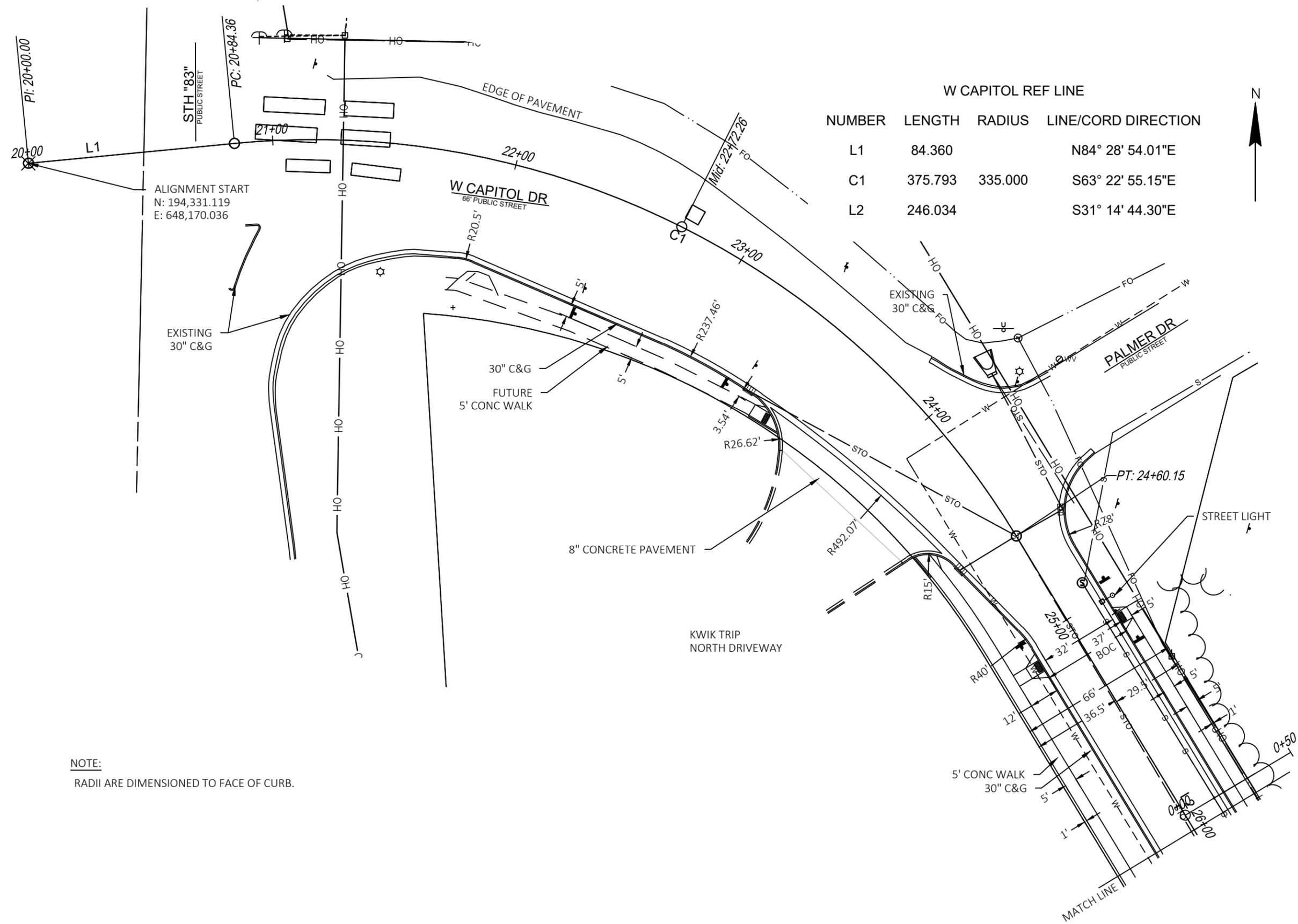


LEGEND

-  PAVEMENT REMOVAL AREA
-  CONCRETE PAVEMENT REMOVAL
-  CURB REMOVAL
-  SAW CUT ASPHALT



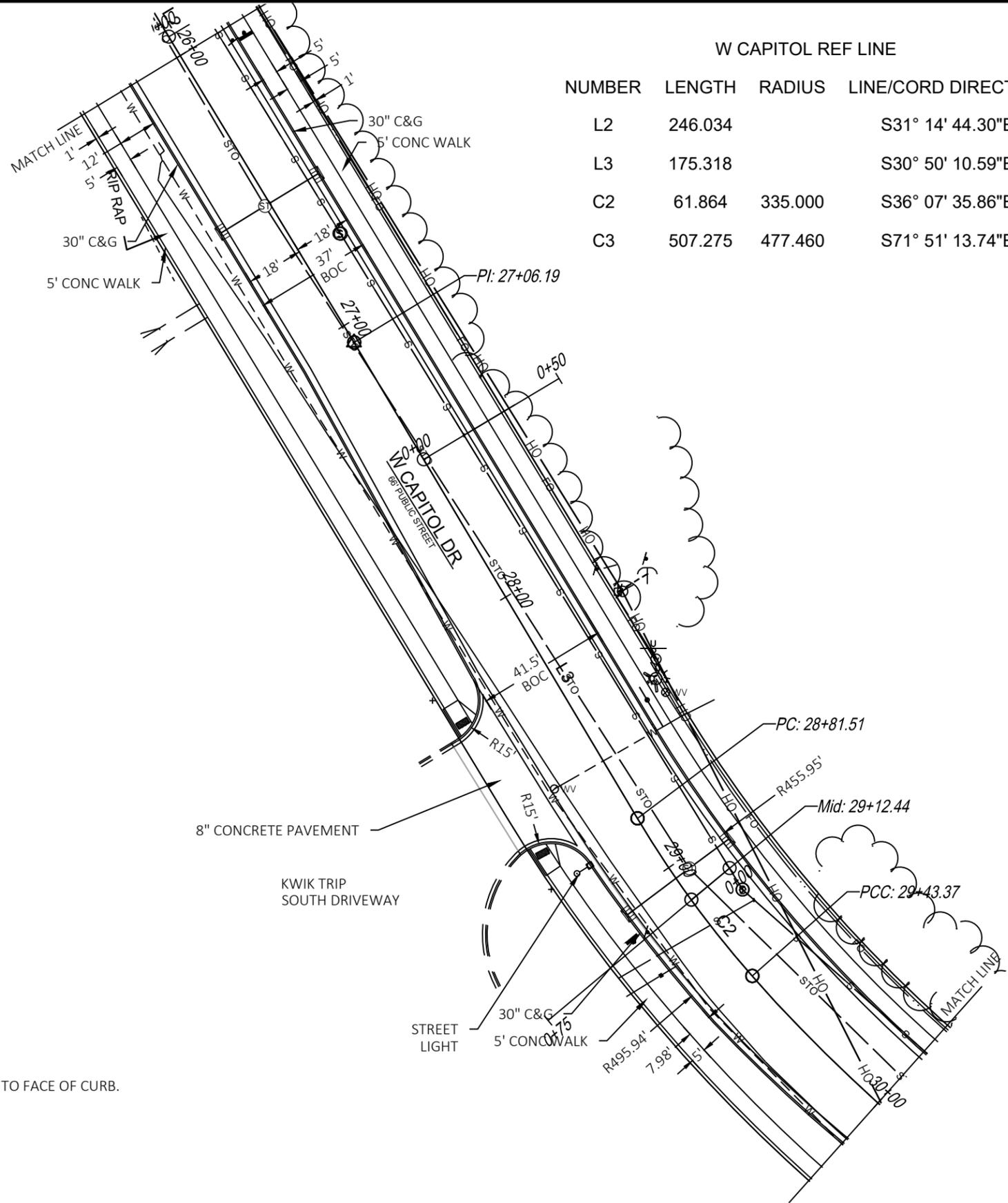
- LEGEND**
-  PAVEMENT REMOVAL AREA
 -  CONCRETE PAVEMENT REMOVAL
 -  CURB REMOVAL
 -  SAW CUT ASPHALT



W CAPITOL REF LINE			
NUMBER	LENGTH	RADIUS	LINE/CORD DIRECTION
L1	84.360		N84° 28' 54.01"E
C1	375.793	335.000	S63° 22' 55.15"E
L2	246.034		S31° 14' 44.30"E



NOTE:
RADI ARE DIMENSIONED TO FACE OF CURB.



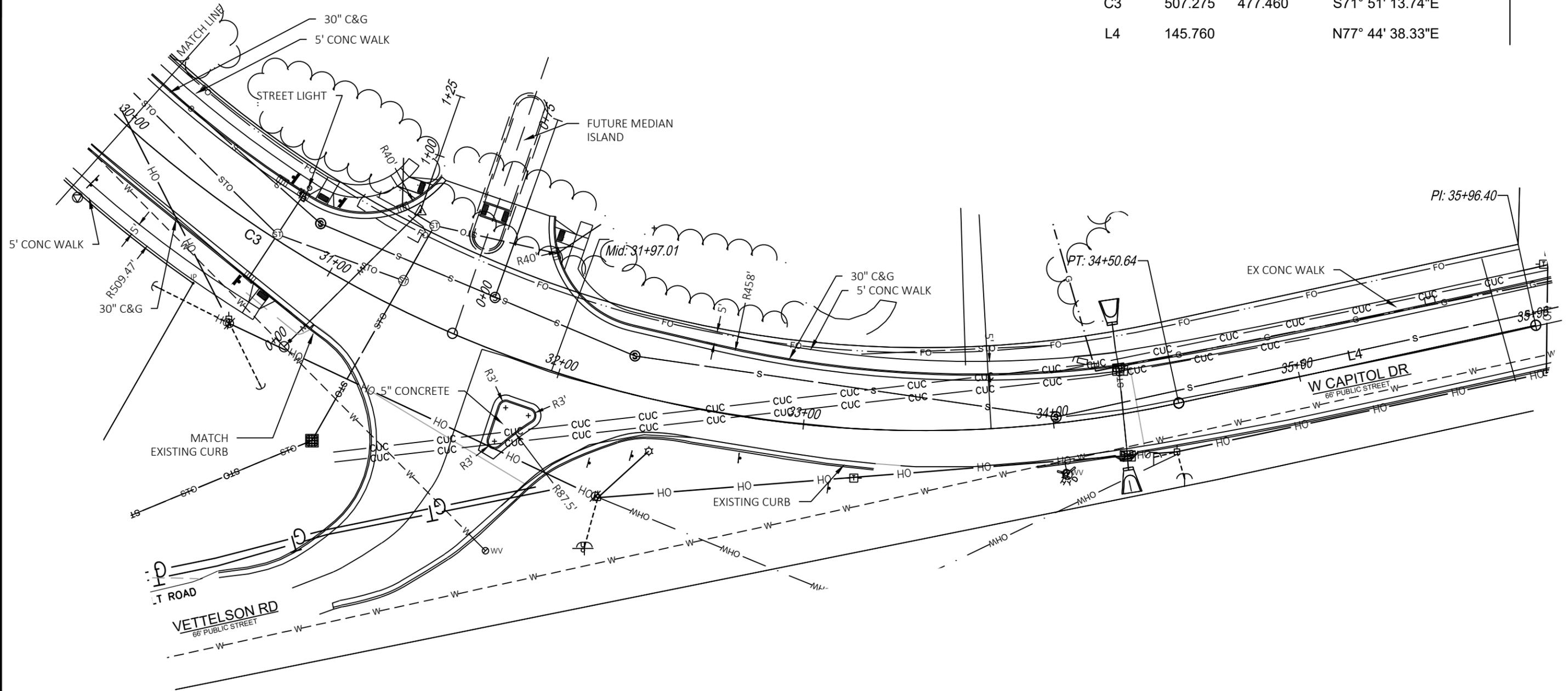
W CAPITOL REF LINE

NUMBER	LENGTH	RADIUS	LINE/CORD DIRECTION
L2	246.034		S31° 14' 44.30"E
L3	175.318		S30° 50' 10.59"E
C2	61.864	335.000	S36° 07' 35.86"E
C3	507.275	477.460	S71° 51' 13.74"E

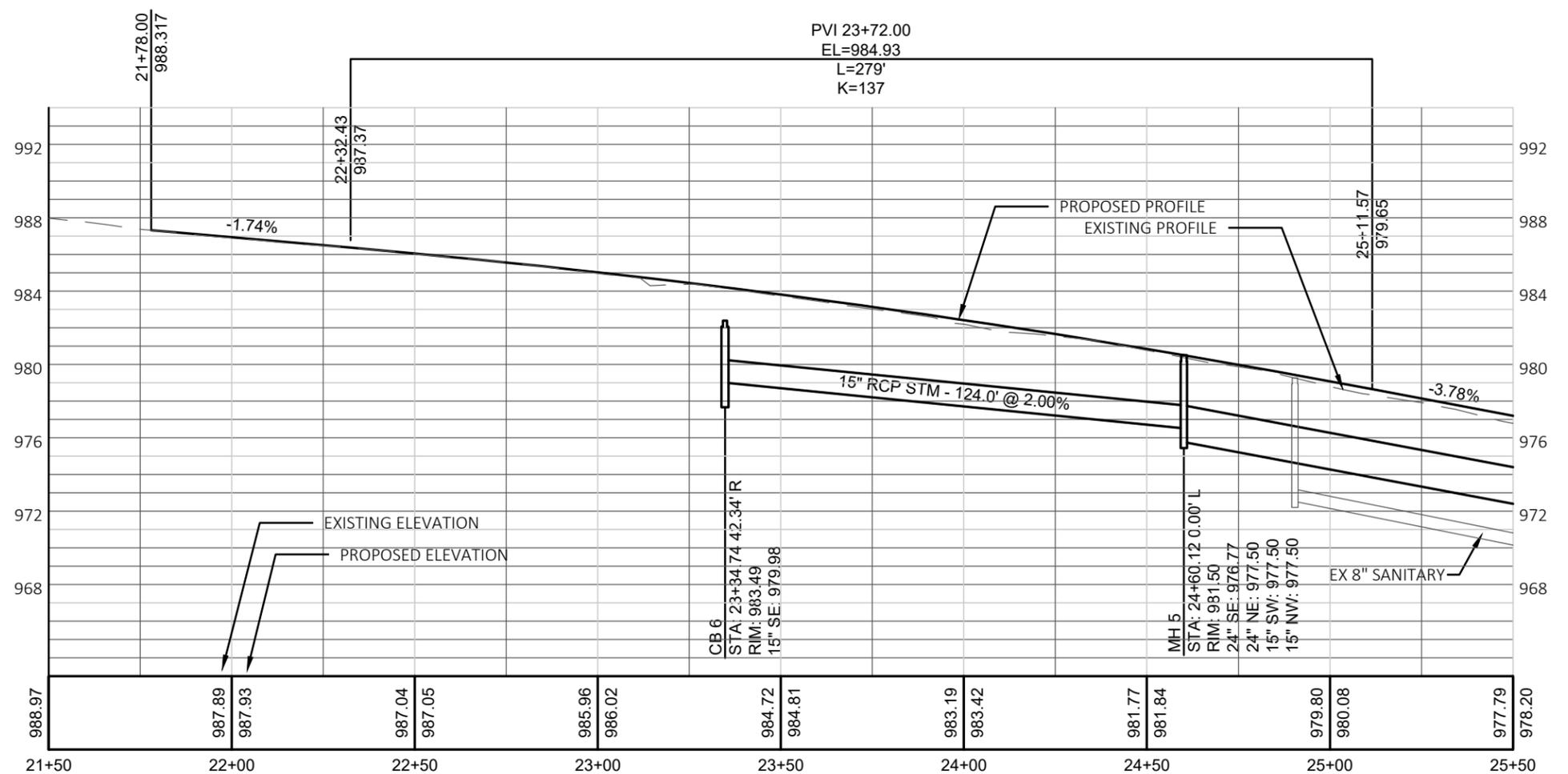
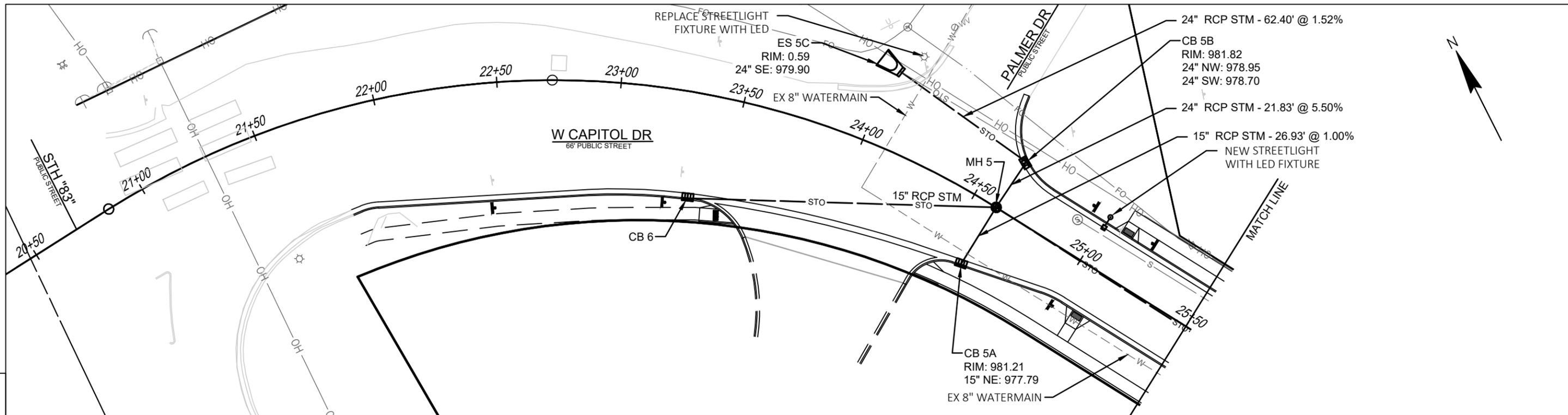
NOTE:
RADI ARE DIMENSIONED TO FACE OF CURB.

W CAPITOL REF LINE

NUMBER	LENGTH	RADIUS	LINE/CORD DIRECTION
C3	507.275	477.460	S71° 51' 13.74"E
L4	145.760		N77° 44' 38.33"E

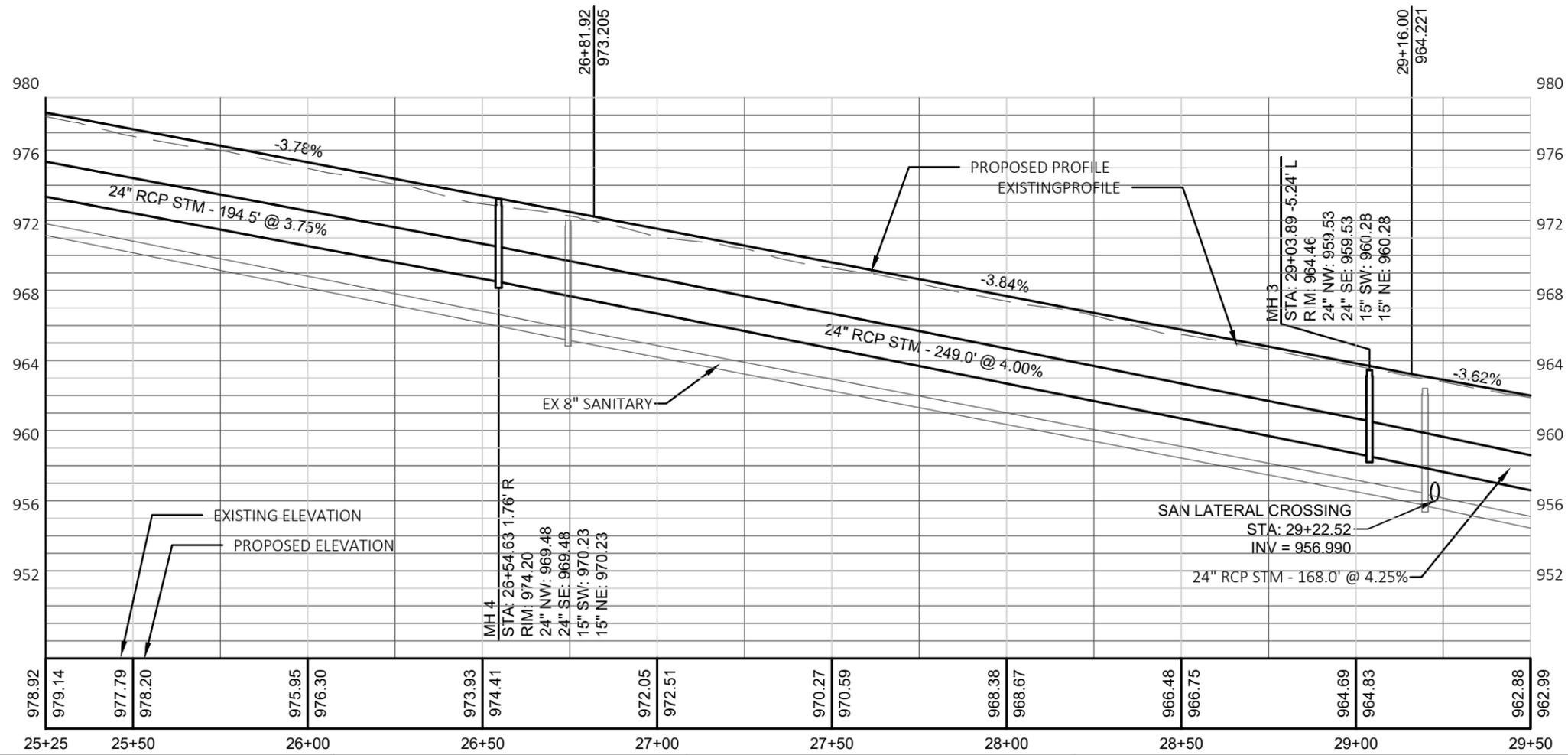
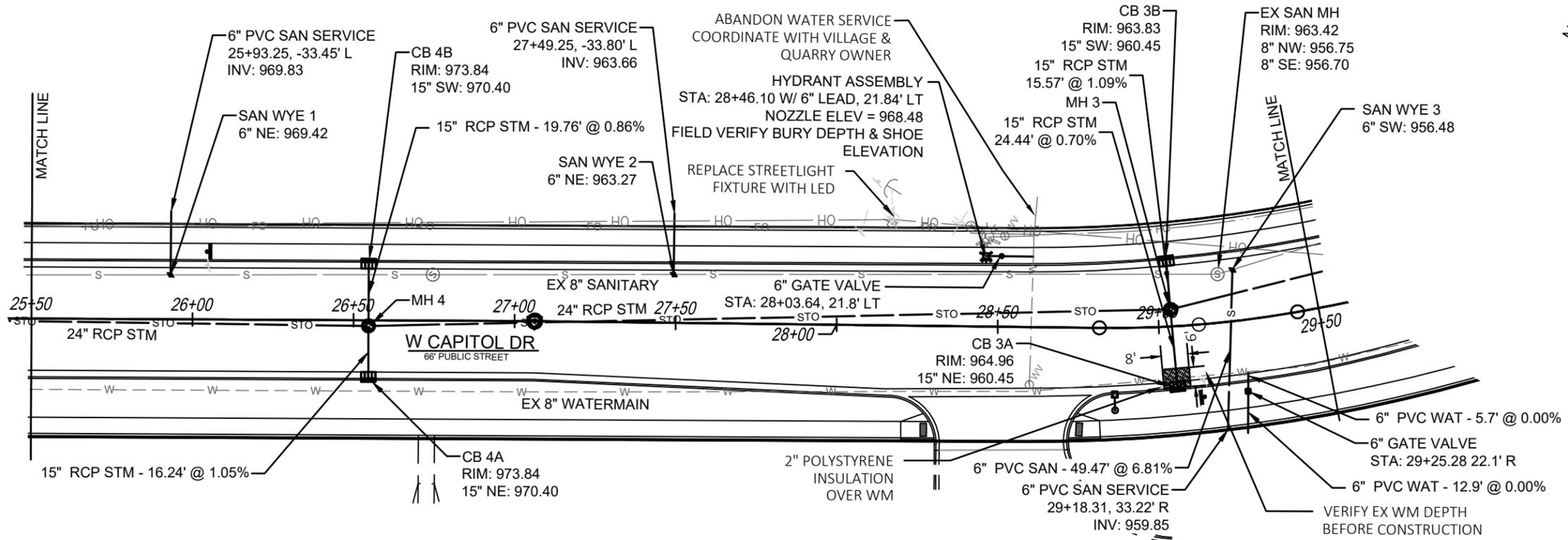


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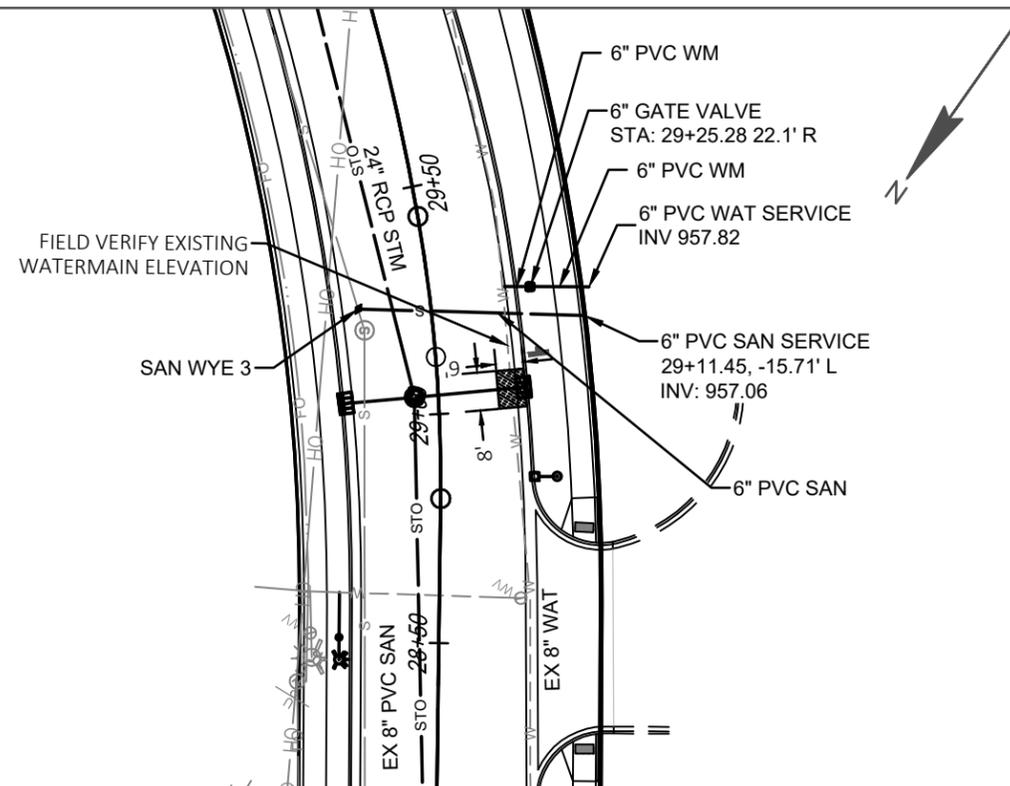
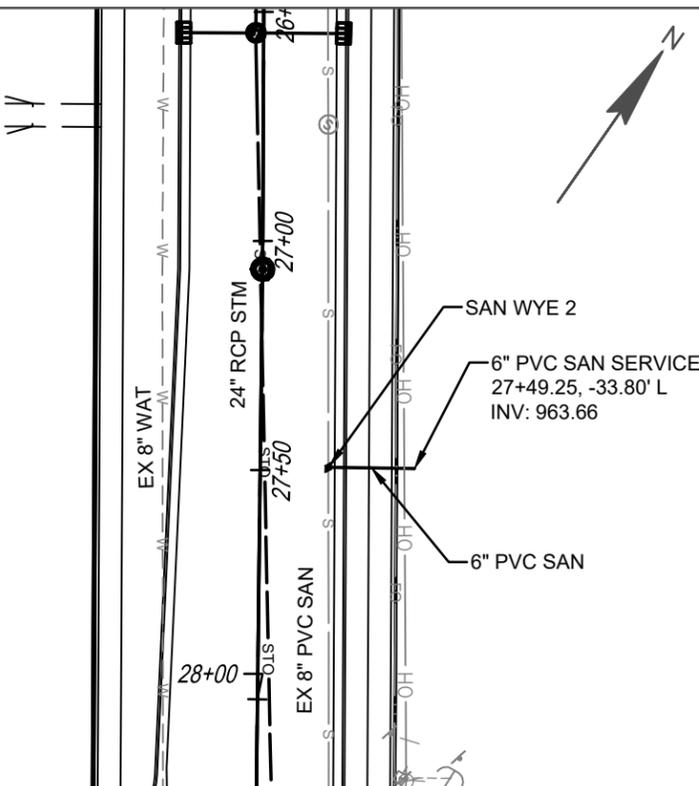
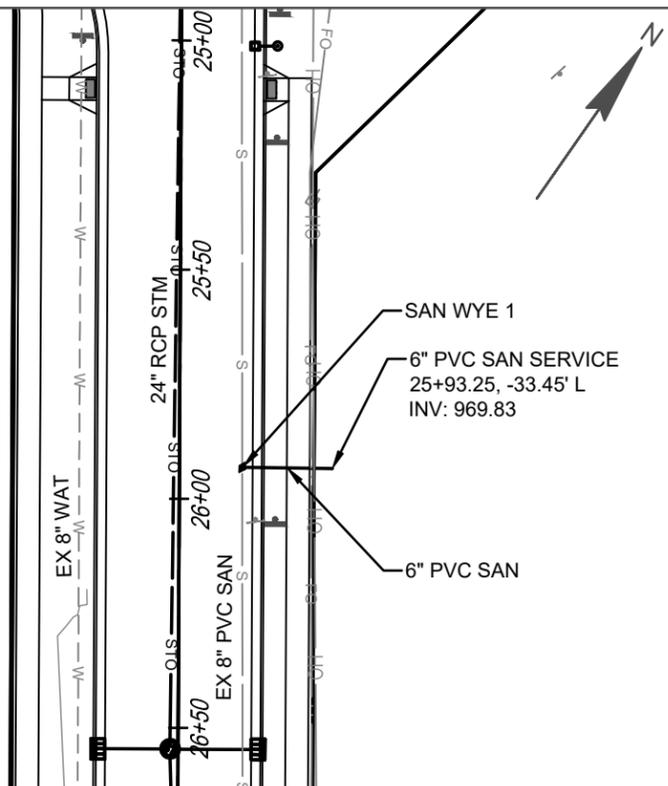


PROJECT NO: 490823 HWY: STH 83 COUNTY: WAUKESHA PLAN AND PROFILE: ROADWAY & STORM PLAN & PROFILE SHEET 12 E

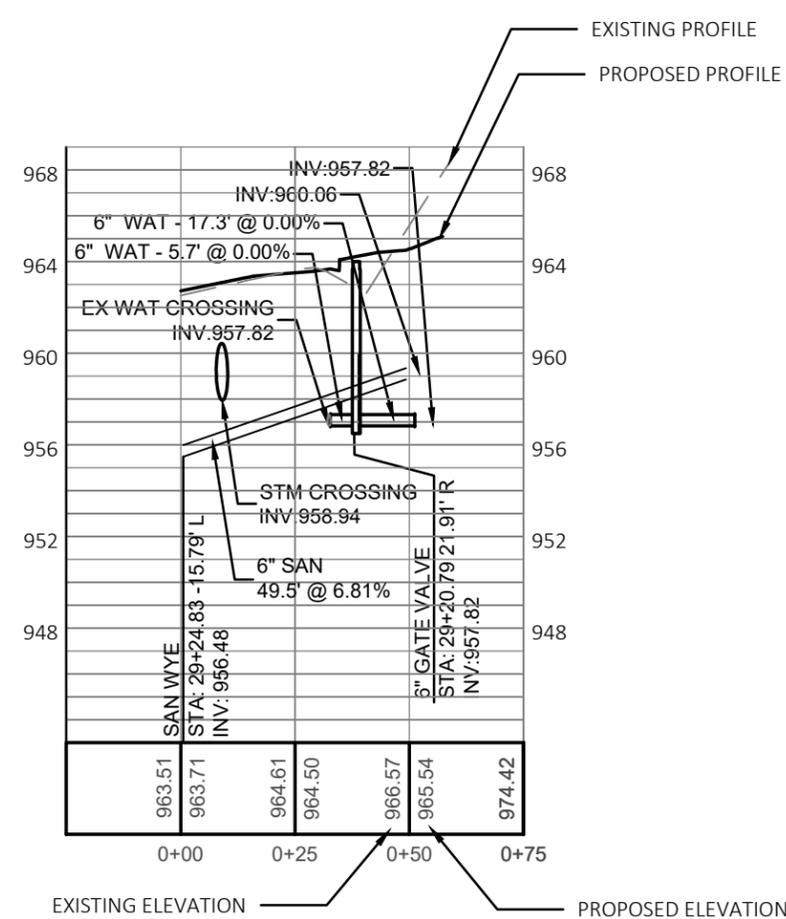
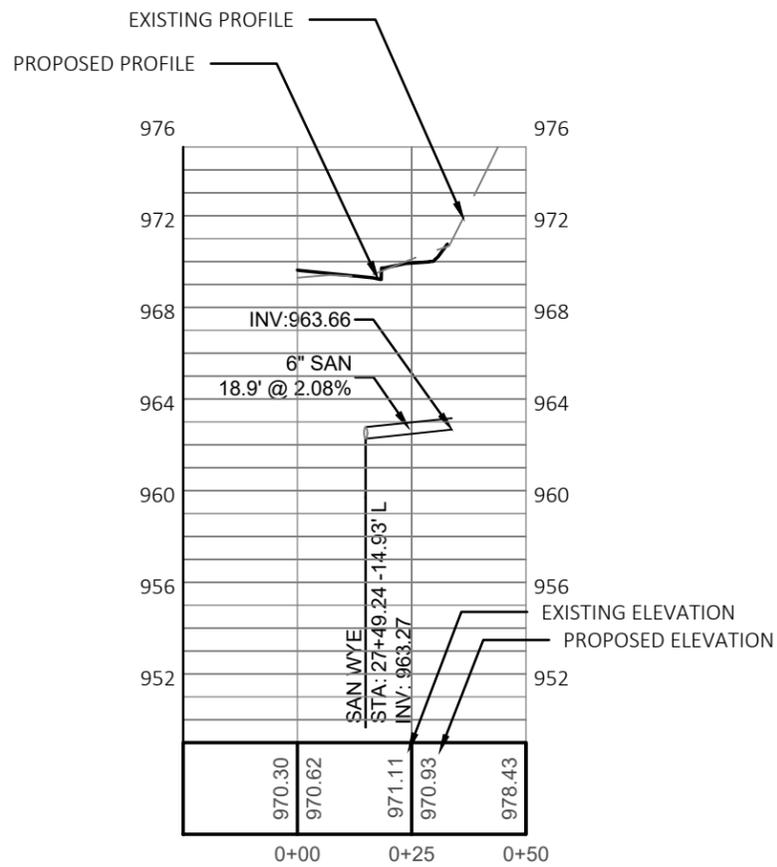
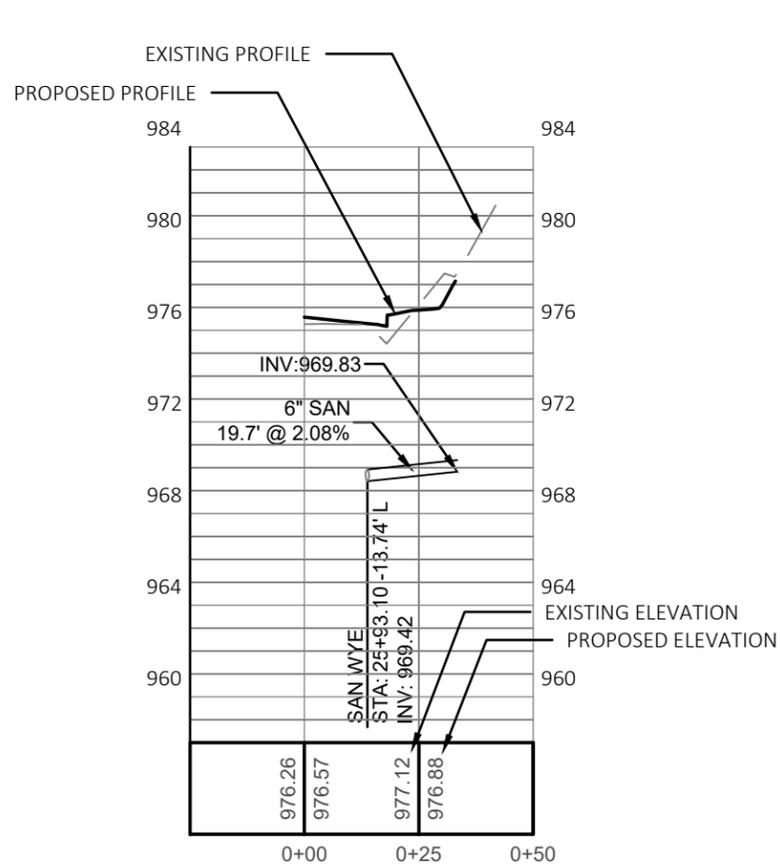
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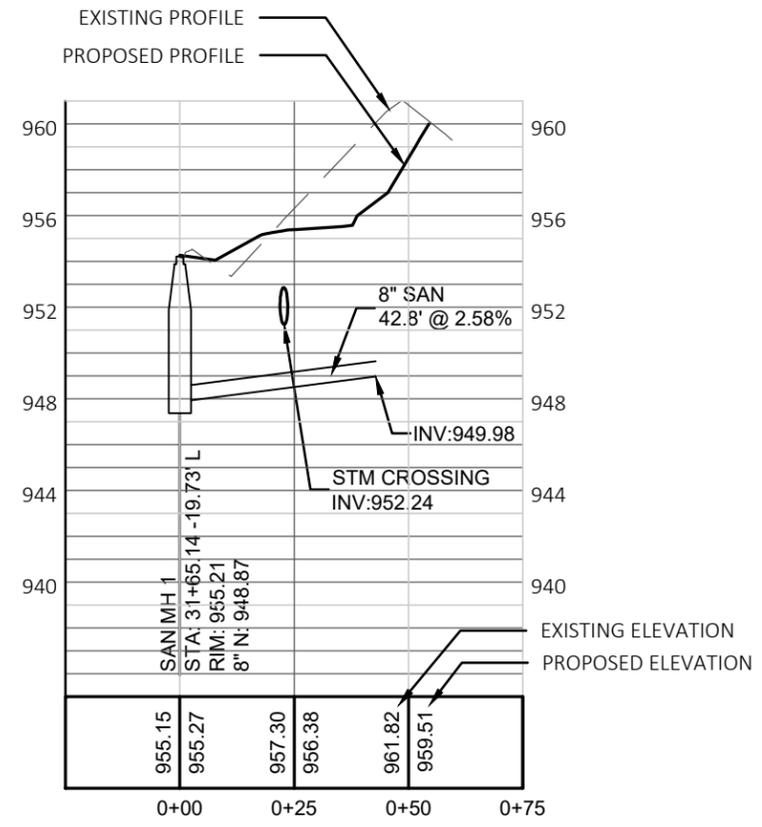
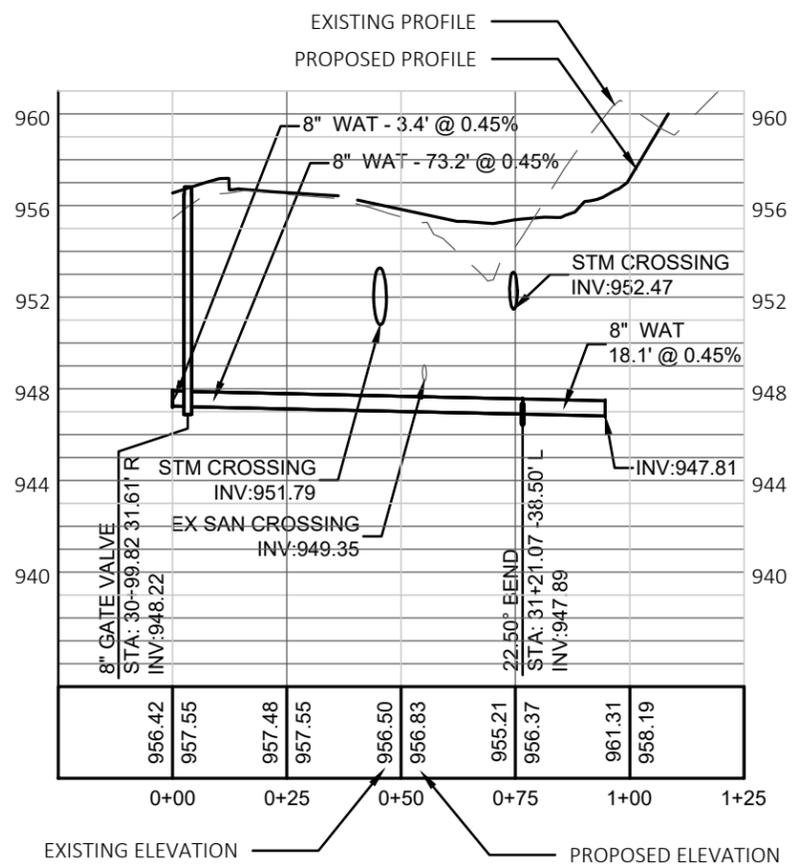
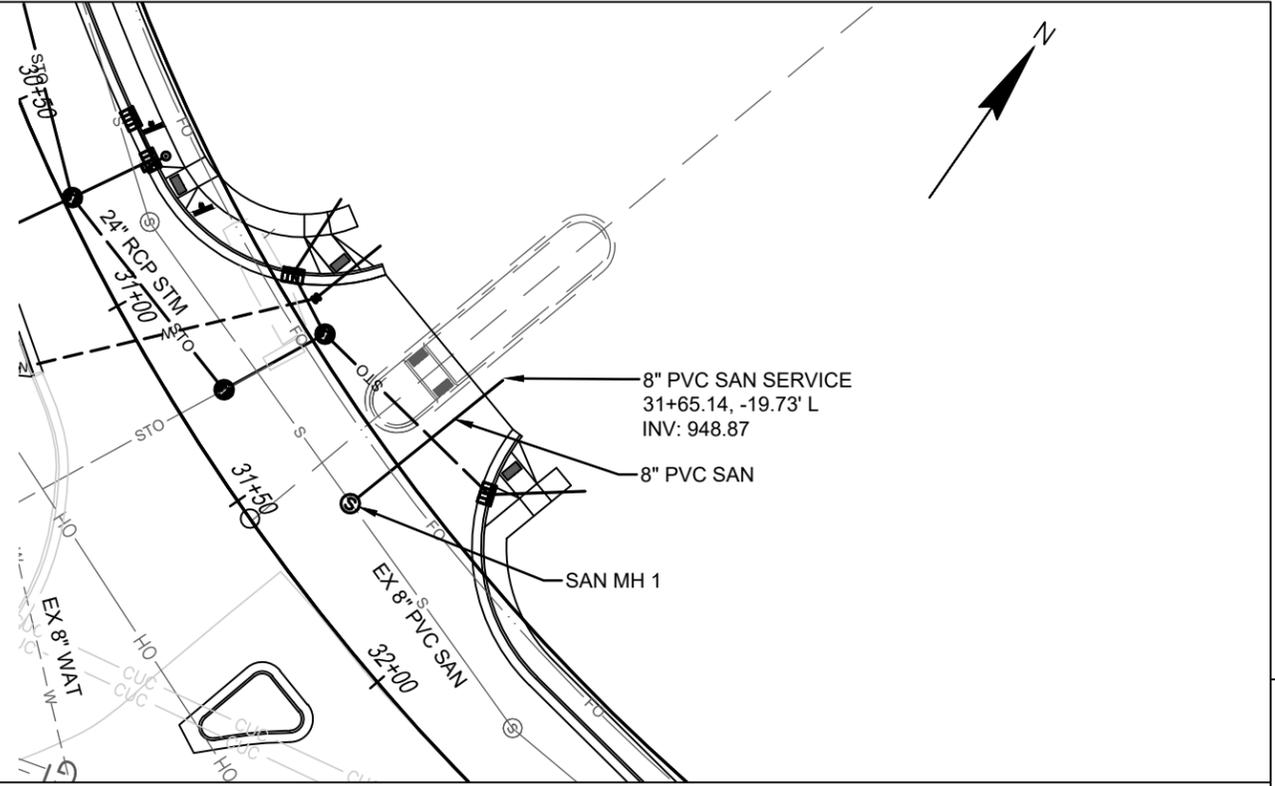
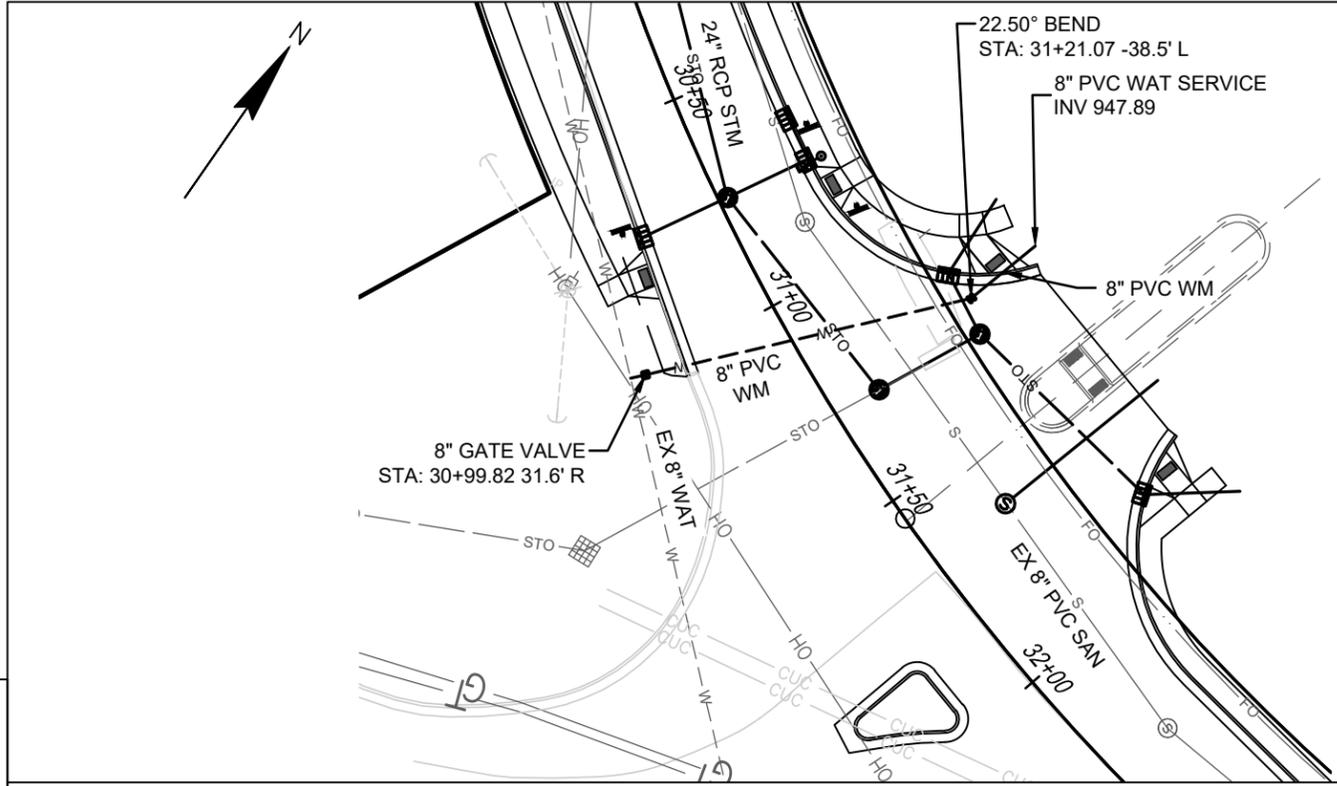


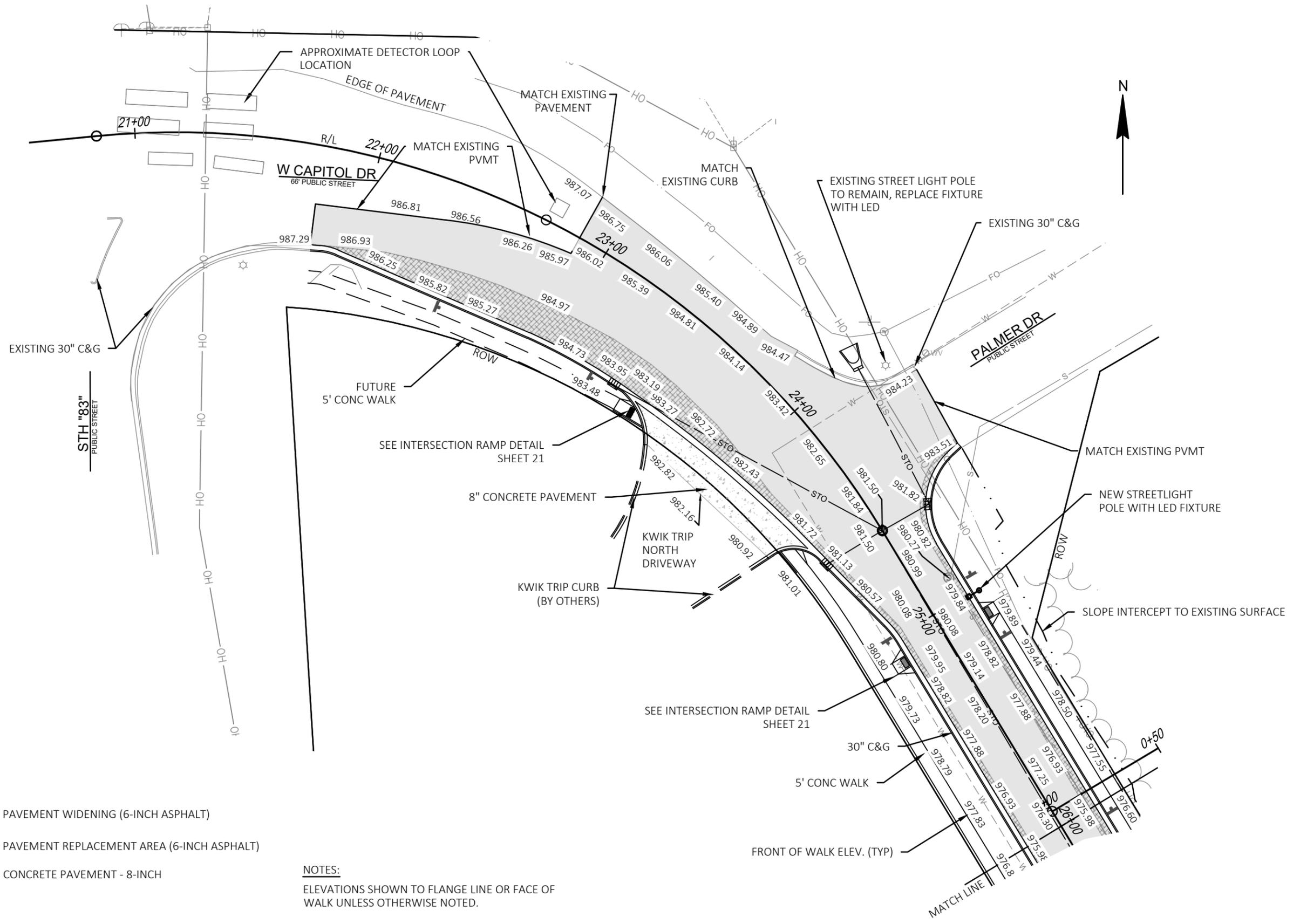
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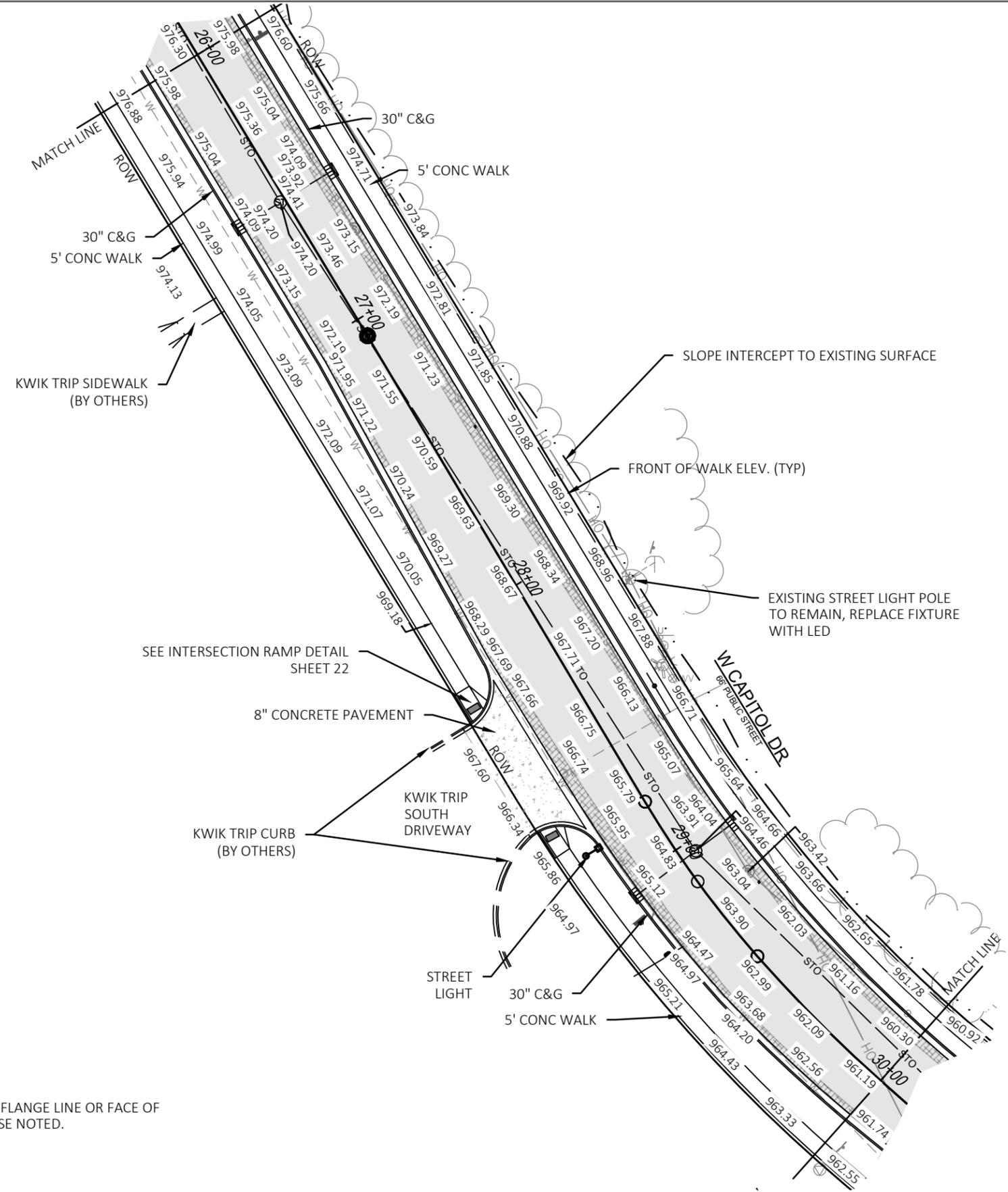


LEGEND

-  PAVEMENT WIDENING (6-INCH ASPHALT)
-  PAVEMENT REPLACEMENT AREA (6-INCH ASPHALT)
-  CONCRETE PAVEMENT - 8-INCH

NOTES:

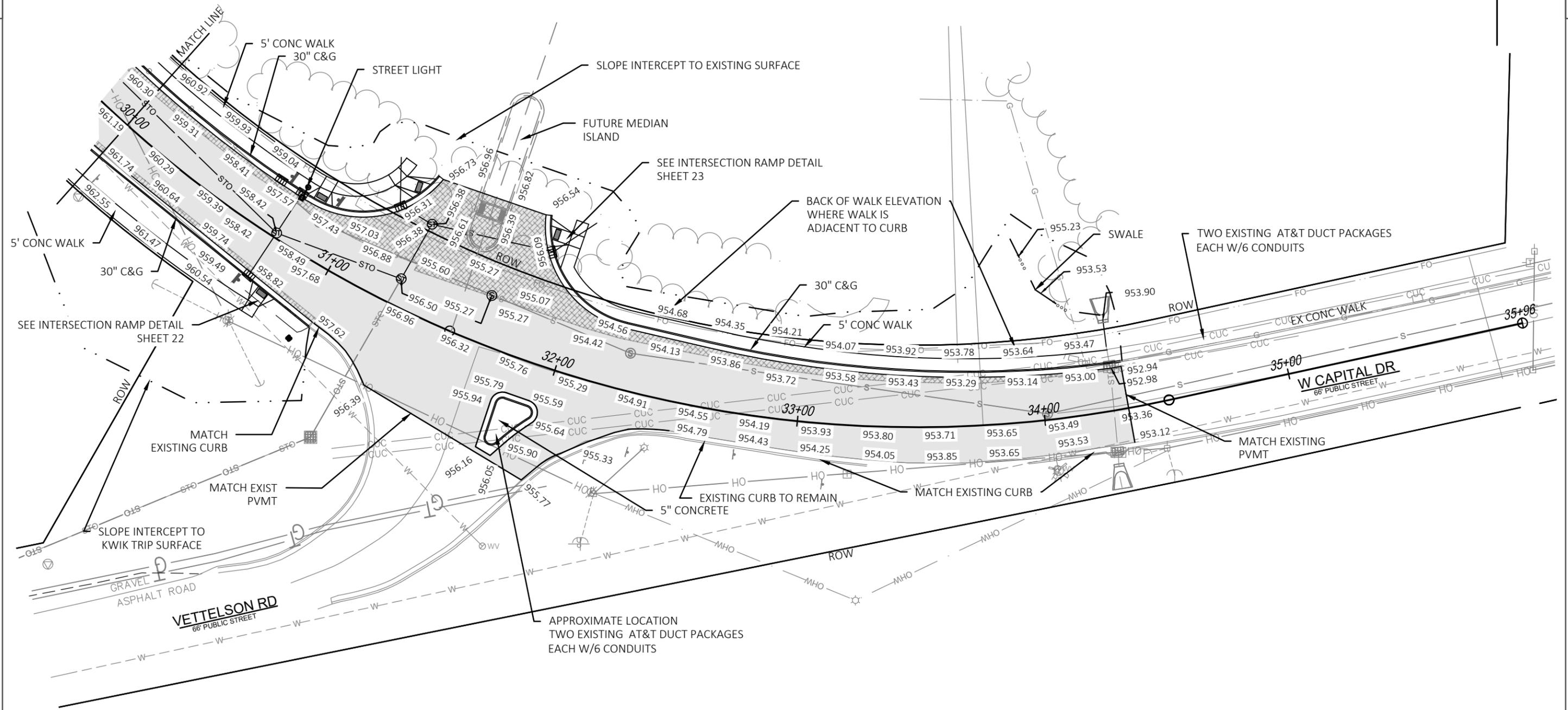
ELEVATIONS SHOWN TO FLANGE LINE OR FACE OF WALK UNLESS OTHERWISE NOTED.



NOTES:
 ELEVATIONS SHOWN TO FLANGE LINE OR FACE OF WALK UNLESS OTHERWISE NOTED.

LEGEND

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-  PAVEMENT REPLACEMENT AREA (6-INCH ASPHALT)
-  CONCRETE PAVEMENT - 8-INCH



LEGEND

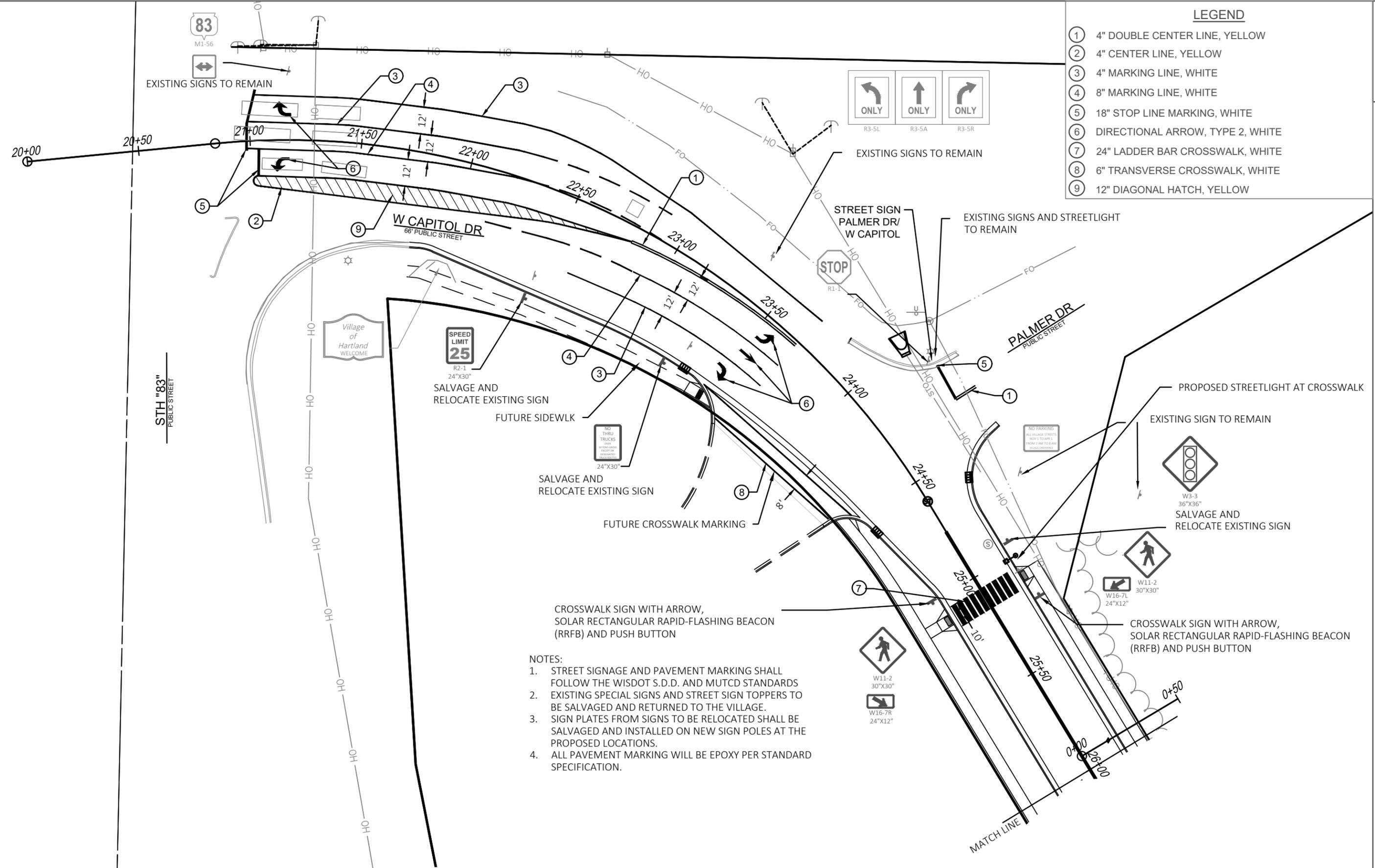
-  PAVEMENT WIDENING (6-INCH ASPHALT)
-  PAVEMENT REPLACEMENT AREA (6-INCH ASPHALT)
-  CONCRETE PAVEMENT - 8-INCH

NOTES:

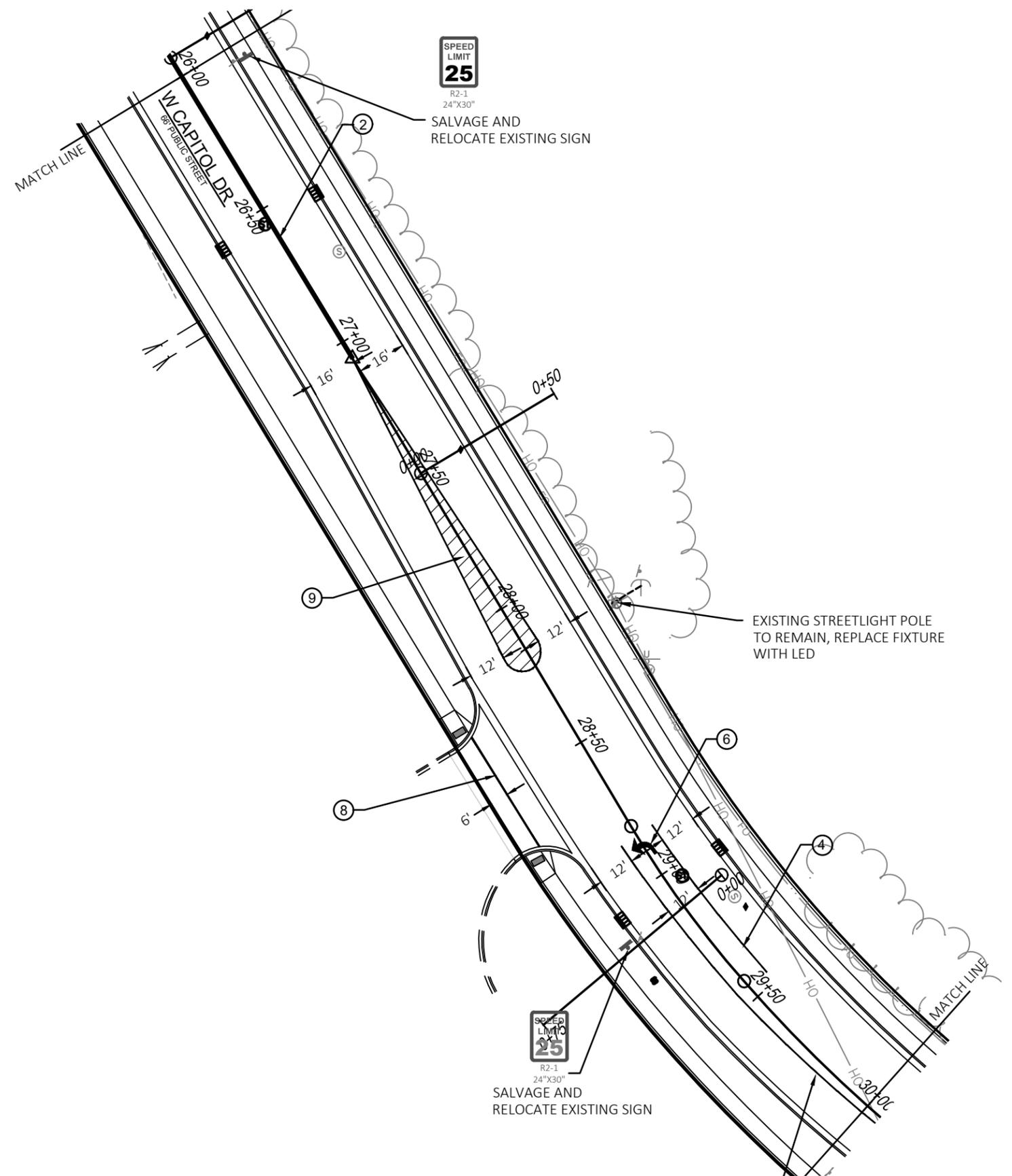
ELEVATIONS SHOWN TO FLANGE LINE OR FACE OF WALK UNLESS OTHERWISE NOTED.

LEGEND

- ① 4" DOUBLE CENTER LINE, YELLOW
- ② 4" CENTER LINE, YELLOW
- ③ 4" MARKING LINE, WHITE
- ④ 8" MARKING LINE, WHITE
- ⑤ 18" STOP LINE MARKING, WHITE
- ⑥ DIRECTIONAL ARROW, TYPE 2, WHITE
- ⑦ 24" LADDER BAR CROSSWALK, WHITE
- ⑧ 6" TRANSVERSE CROSSWALK, WHITE
- ⑨ 12" DIAGONAL HATCH, YELLOW



- NOTES:
1. STREET SIGNAGE AND PAVEMENT MARKING SHALL FOLLOW THE WISDOT S.D.D. AND MUTCD STANDARDS
 2. EXISTING SPECIAL SIGNS AND STREET SIGN TOPPERS TO BE SALVAGED AND RETURNED TO THE VILLAGE.
 3. SIGN PLATES FROM SIGNS TO BE RELOCATED SHALL BE SALVAGED AND INSTALLED ON NEW SIGN POLES AT THE PROPOSED LOCATIONS.
 4. ALL PAVEMENT MARKING WILL BE EPOXY PER STANDARD SPECIFICATION.



LEGEND

- ① 4" DOUBLE CENTER LINE, YELLOW
- ② 4" CENTER LINE, YELLOW
- ③ 4" MARKING LINE, WHITE
- ④ 8" MARKING LINE, WHITE
- ⑤ 18" STOP LINE MARKING, WHITE
- ⑥ DIRECTIONAL ARROW, TYPE 2, WHITE
- ⑦ 24" LADDER BAR CROSSWALK, WHITE
- ⑧ 6" TRANSVERSE CROSSWALK, WHITE
- ⑨ 12" DIAGONAL HATCH, YELLOW



NOTES:

1. STREET SIGNAGE AND PAVEMENT MARKING SHALL FOLLOW THE WISDOT S.D.D. AND MUTCD STANDARDS
2. EXISTING SPECIAL SIGNS AND STREET SIGN TOPPERS TO BE SALVAGED AND RETURNED TO THE VILLAGE.
3. SIGN PLATES FROM SIGNS TO BE RELOCATED SHALL BE SALVAGED AND INSTALLED ON NEW SIGN POLES AT THE PROPOSED LOCATIONS.
4. ALL PAVEMENT MARKING WILL BE EPOXY PER STANDARD SPECIFICATION.



R2-1
24"X30"
SALVAGE AND
RELOCATE EXISTING SIGN

EXISTING STREETLIGHT POLE
TO REMAIN, REPLACE FIXTURE
WITH LED



R2-1
24"X30"
SALVAGE AND
RELOCATE EXISTING SIGN

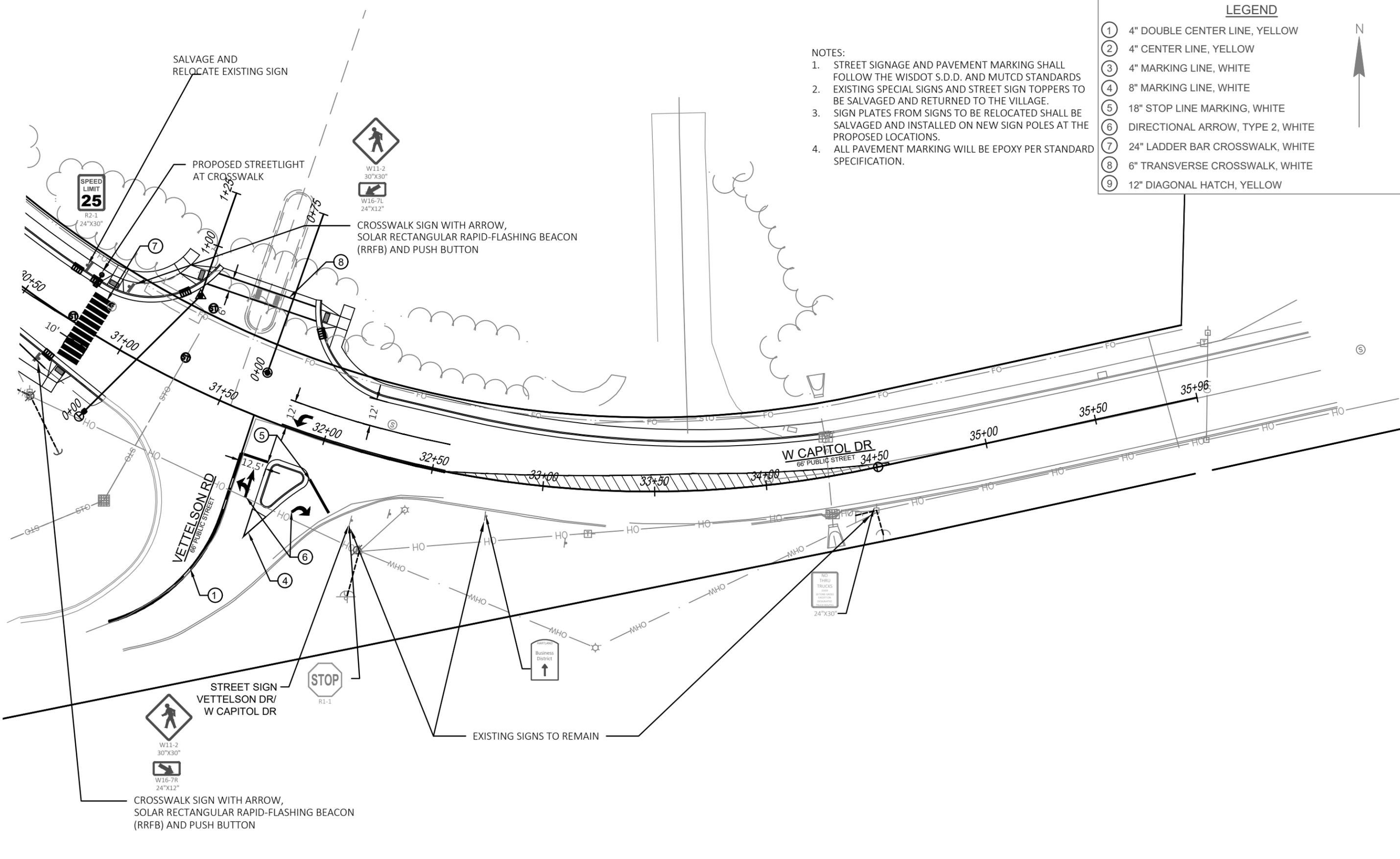
LEGEND

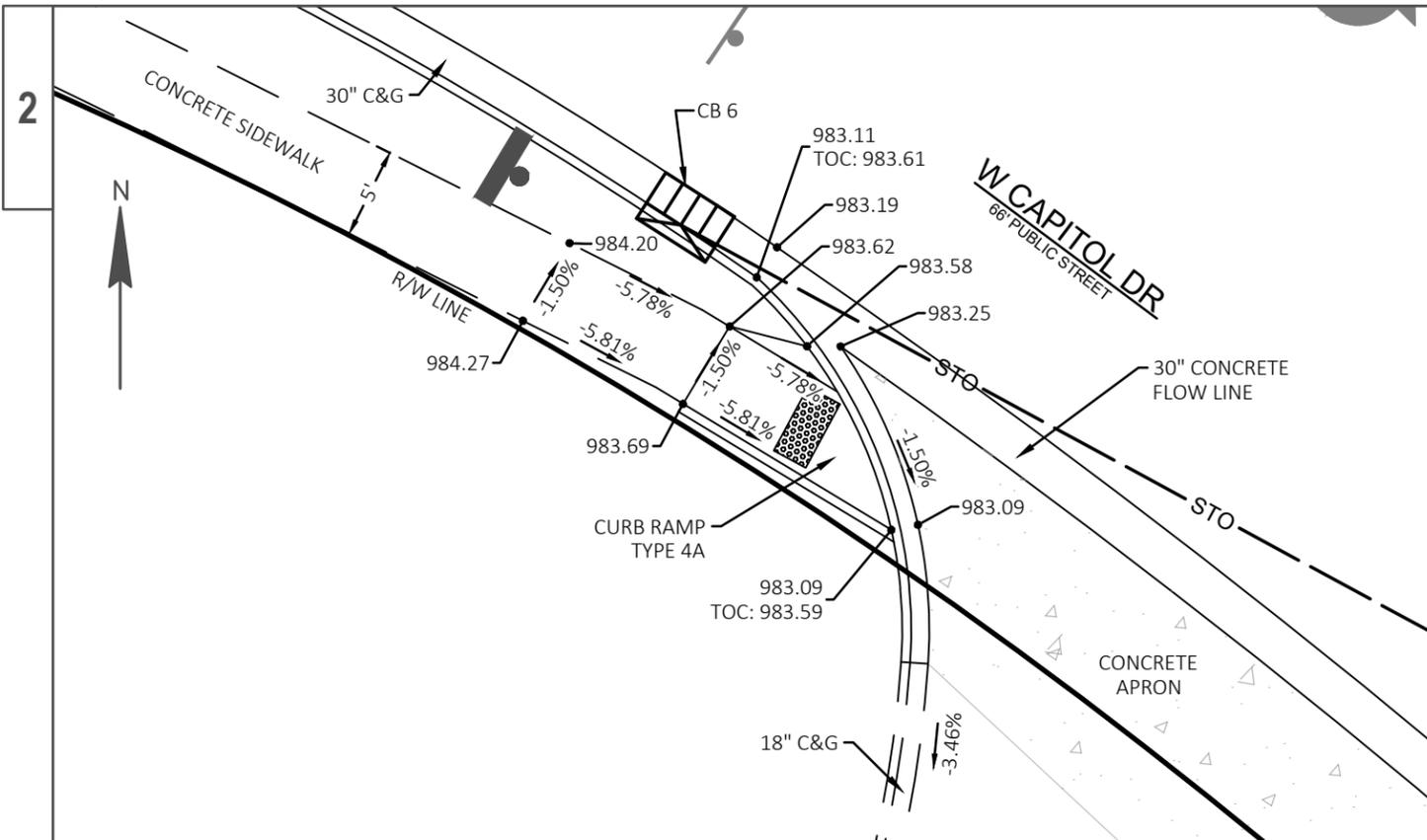
- ① 4" DOUBLE CENTER LINE, YELLOW
- ② 4" CENTER LINE, YELLOW
- ③ 4" MARKING LINE, WHITE
- ④ 8" MARKING LINE, WHITE
- ⑤ 18" STOP LINE MARKING, WHITE
- ⑥ DIRECTIONAL ARROW, TYPE 2, WHITE
- ⑦ 24" LADDER BAR CROSSWALK, WHITE
- ⑧ 6" TRANSVERSE CROSSWALK, WHITE
- ⑨ 12" DIAGONAL HATCH, YELLOW



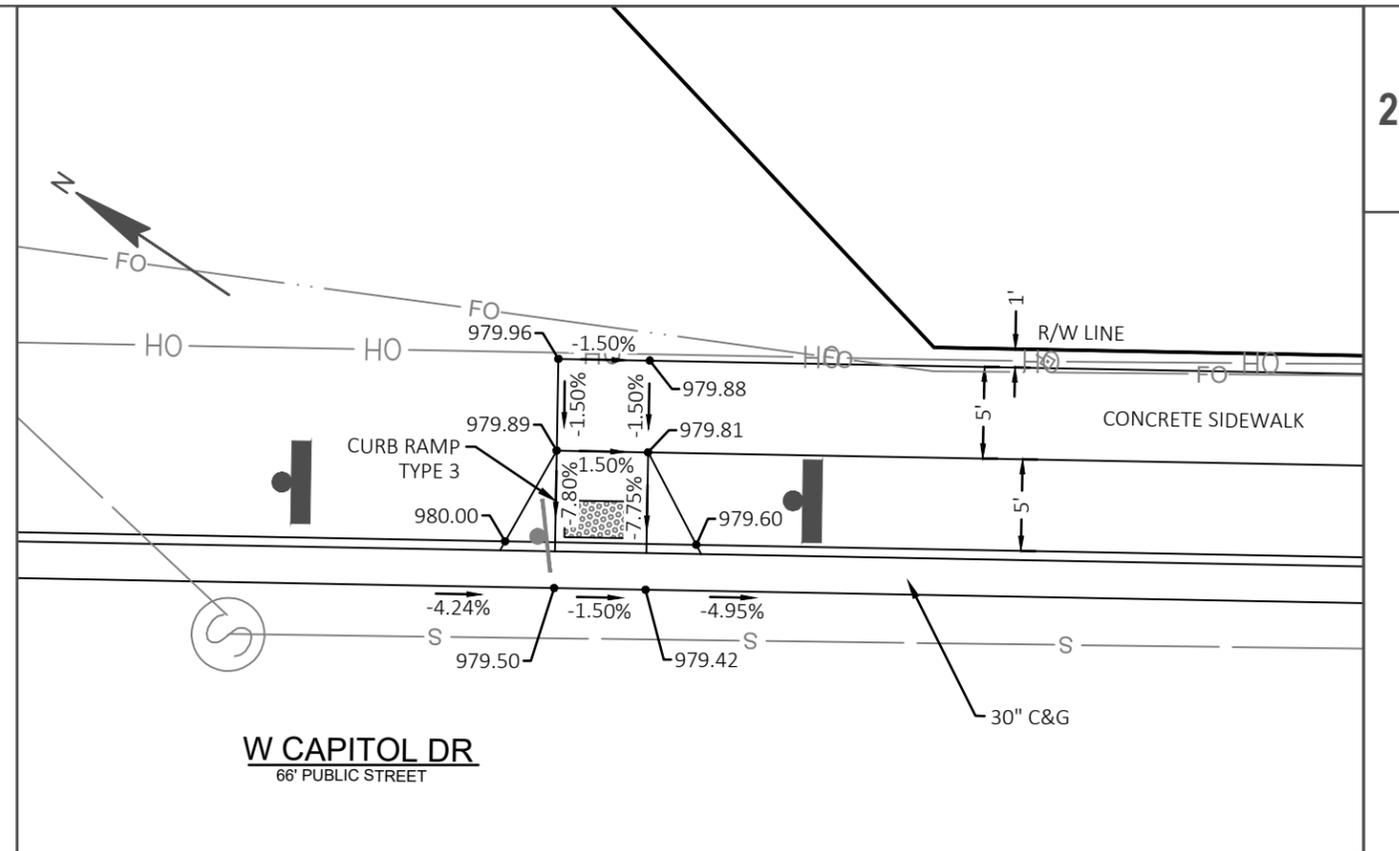
NOTES:

1. STREET SIGNAGE AND PAVEMENT MARKING SHALL FOLLOW THE WISDOT S.D.D. AND MUTCD STANDARDS
2. EXISTING SPECIAL SIGNS AND STREET SIGN TOPPERS TO BE SALVAGED AND RETURNED TO THE VILLAGE.
3. SIGN PLATES FROM SIGNS TO BE RELOCATED SHALL BE SALVAGED AND INSTALLED ON NEW SIGN POLES AT THE PROPOSED LOCATIONS.
4. ALL PAVEMENT MARKING WILL BE EPOXY PER STANDARD SPECIFICATION.

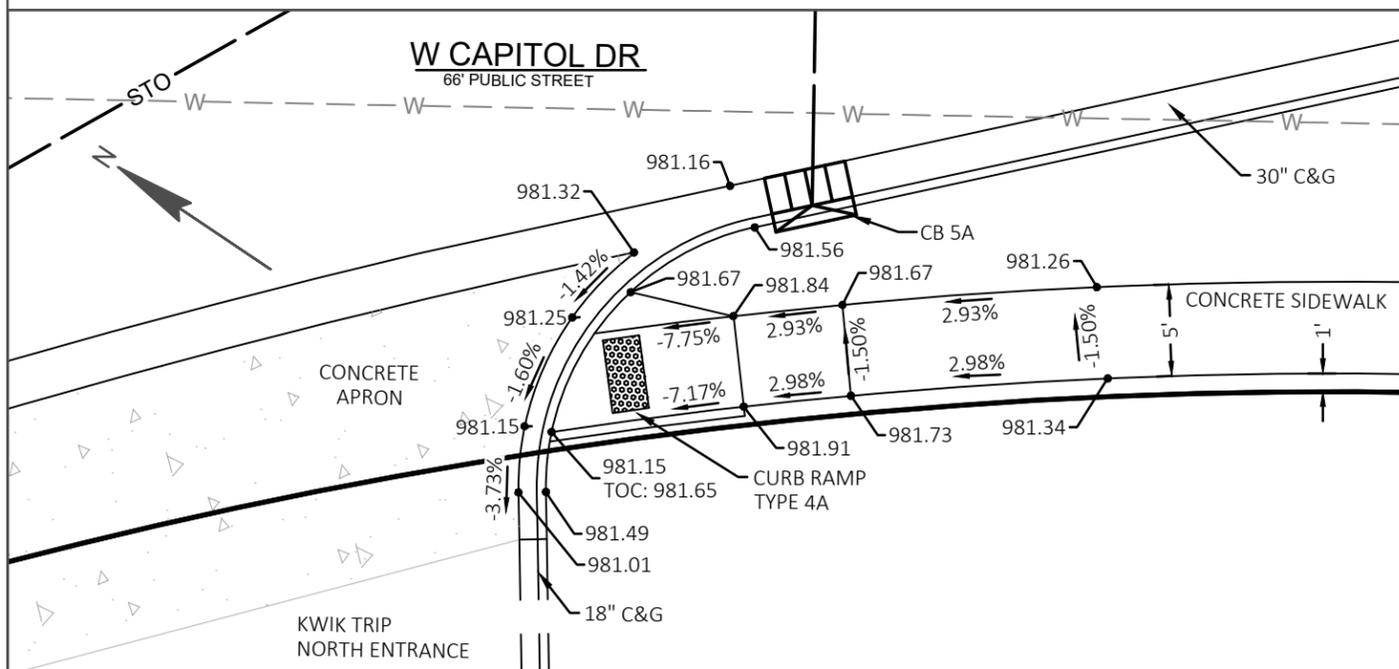




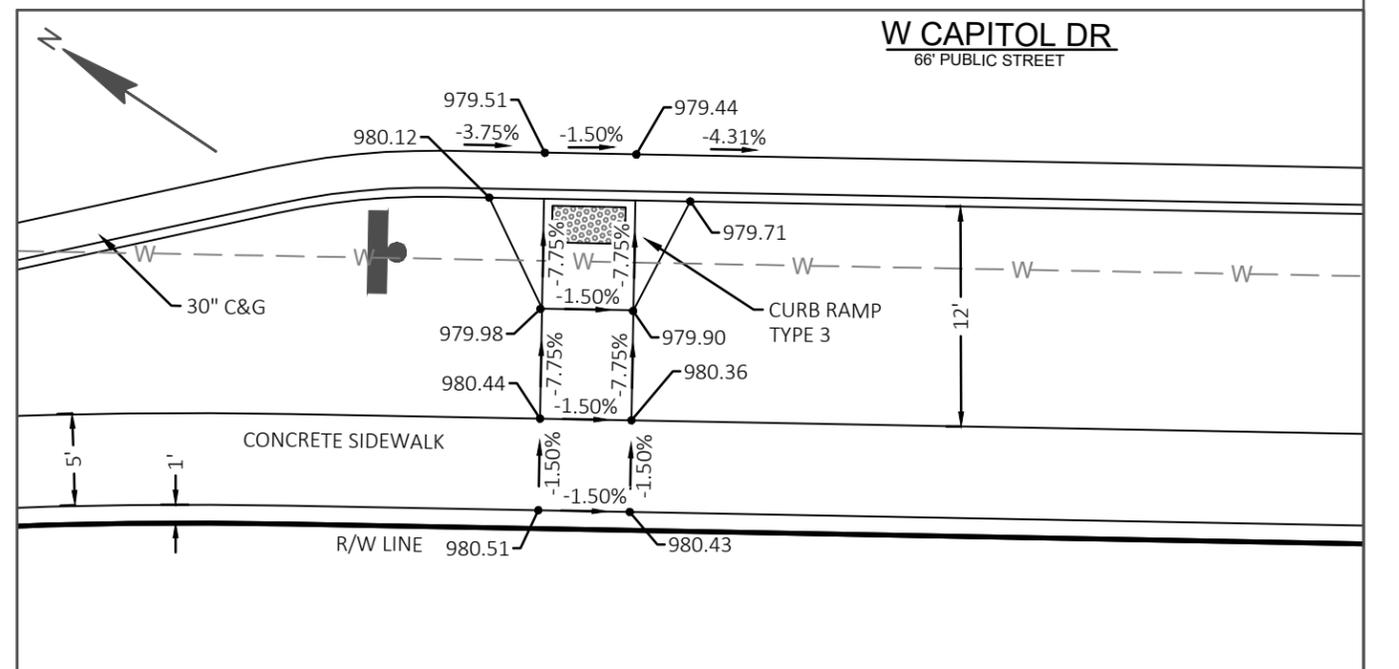
W. CAPITOL DRIVE
CURB RAMP
STA: 23+47.51, 47' RT



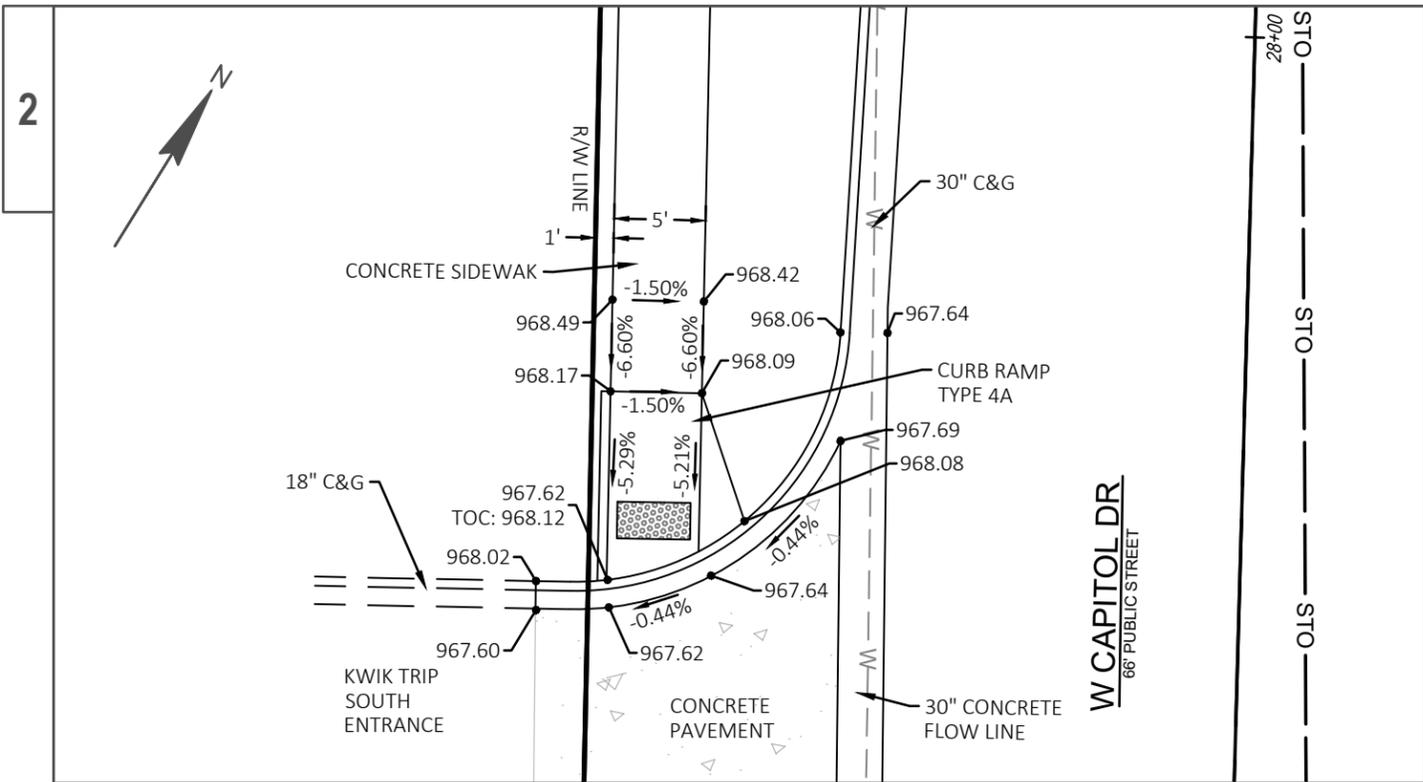
W. CAPITOL DRIVE
CURB RAMP
STA: 25+10.66, 21' LT



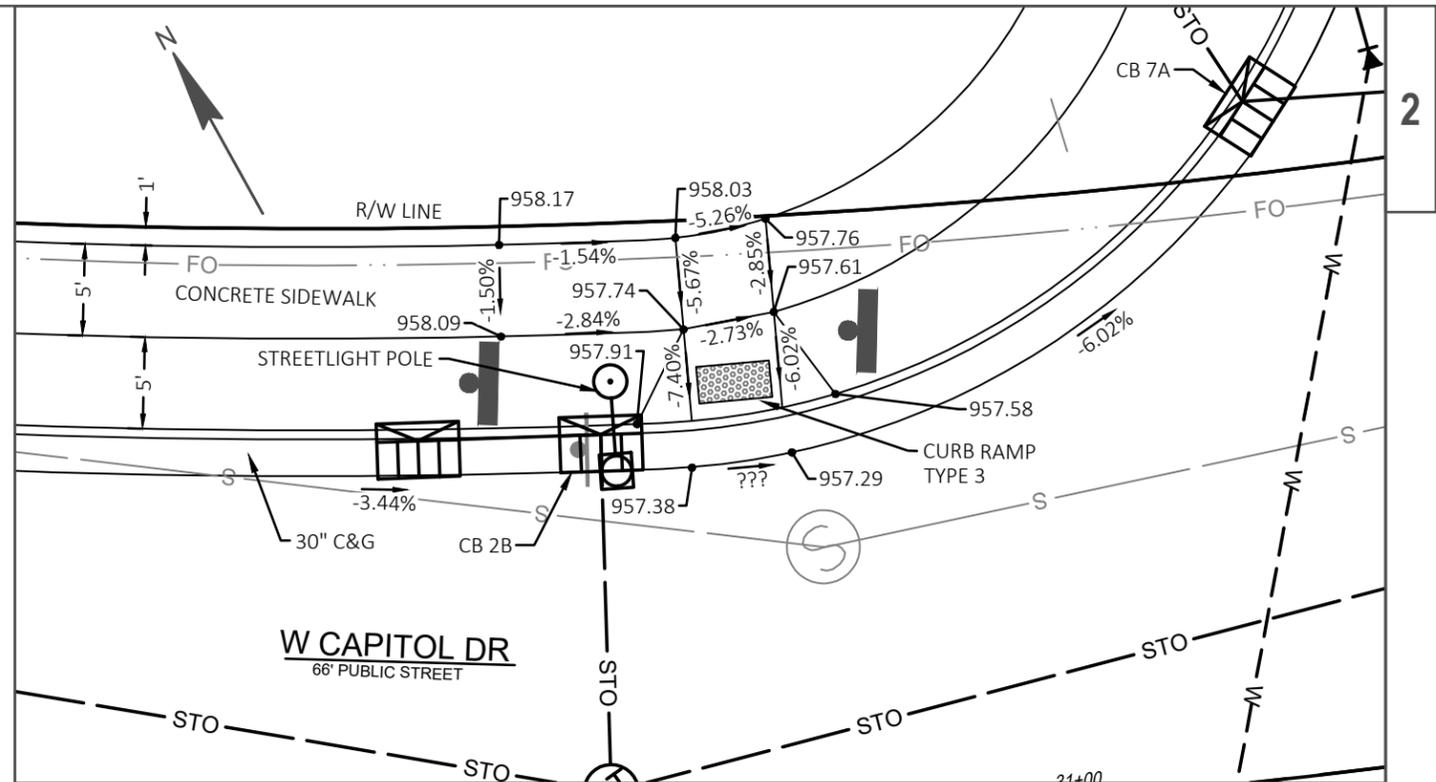
W. CAPITOL DRIVE
CURB RAMP
STA: 24+49.89, 36.5' RT



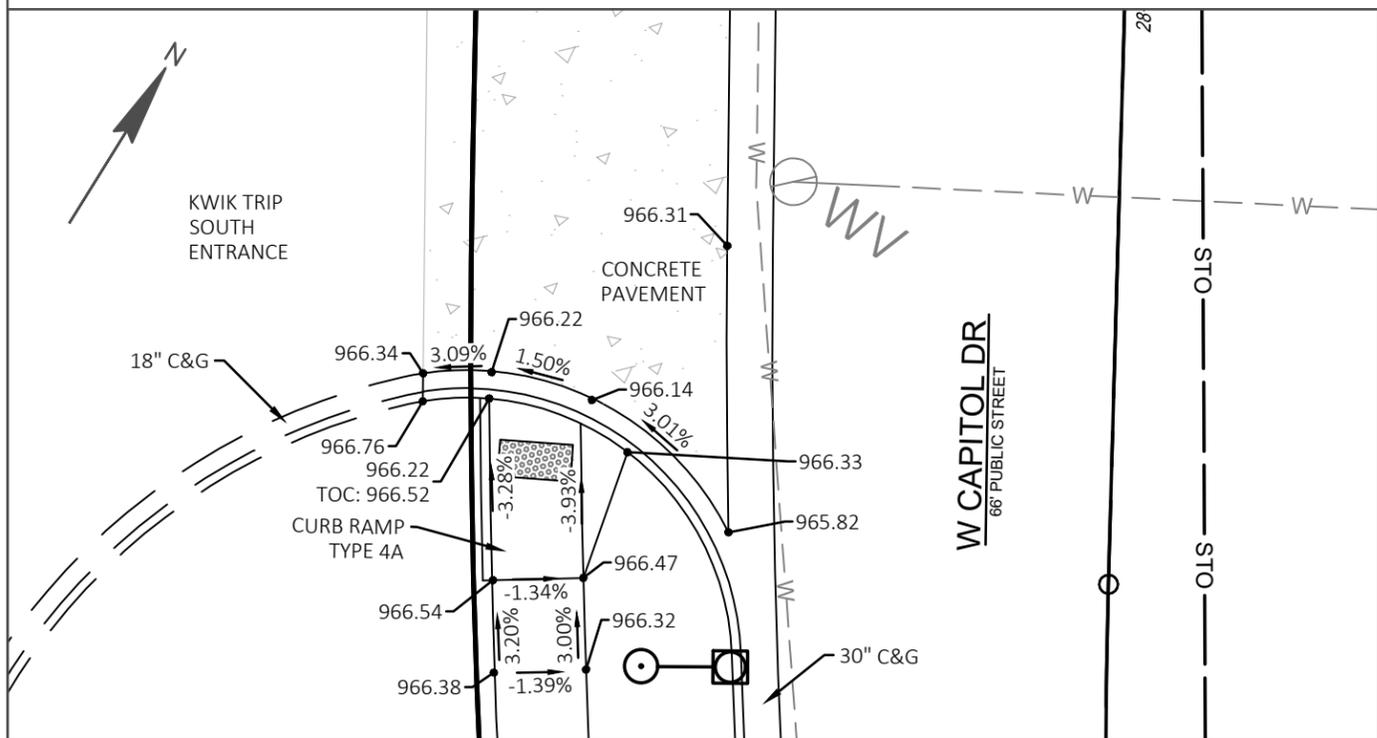
W. CAPITOL DRIVE
CURB RAMP
STA: 25+10.75, 21' RT



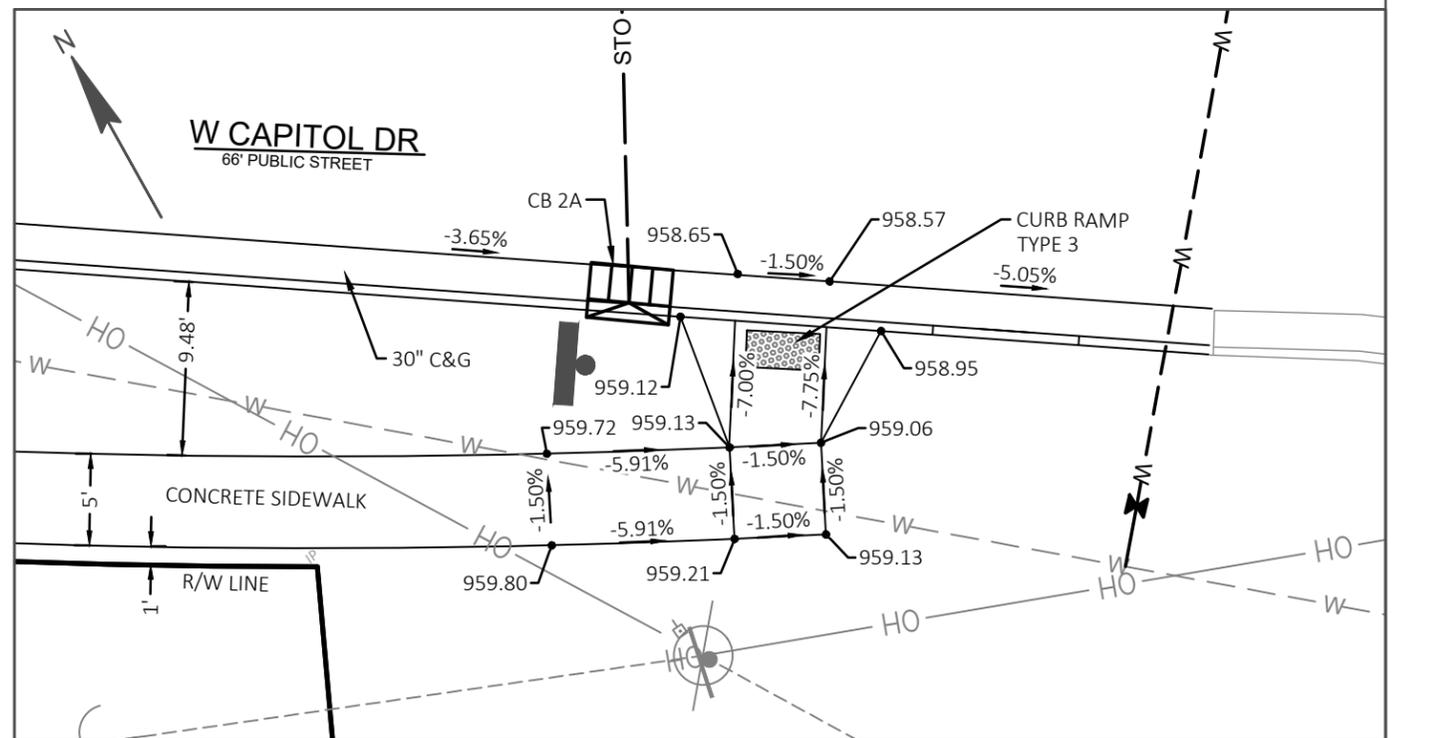
W. CAPITOL DRIVE
CURB RAMP
STA: 28+26.21, 32' RT



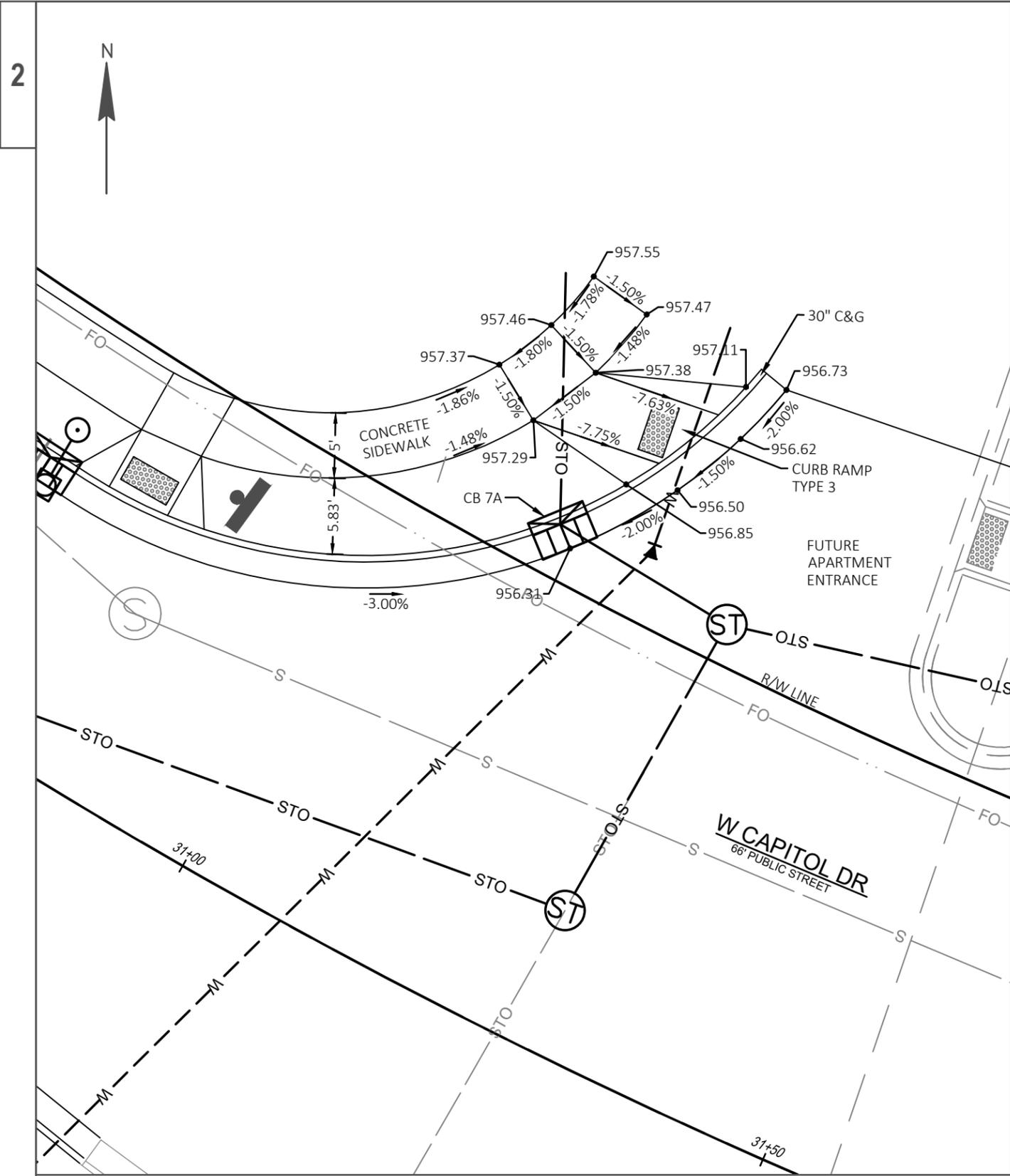
W. CAPITOL DRIVE
CURB RAMP
STA: 30+82.50, 25' LT



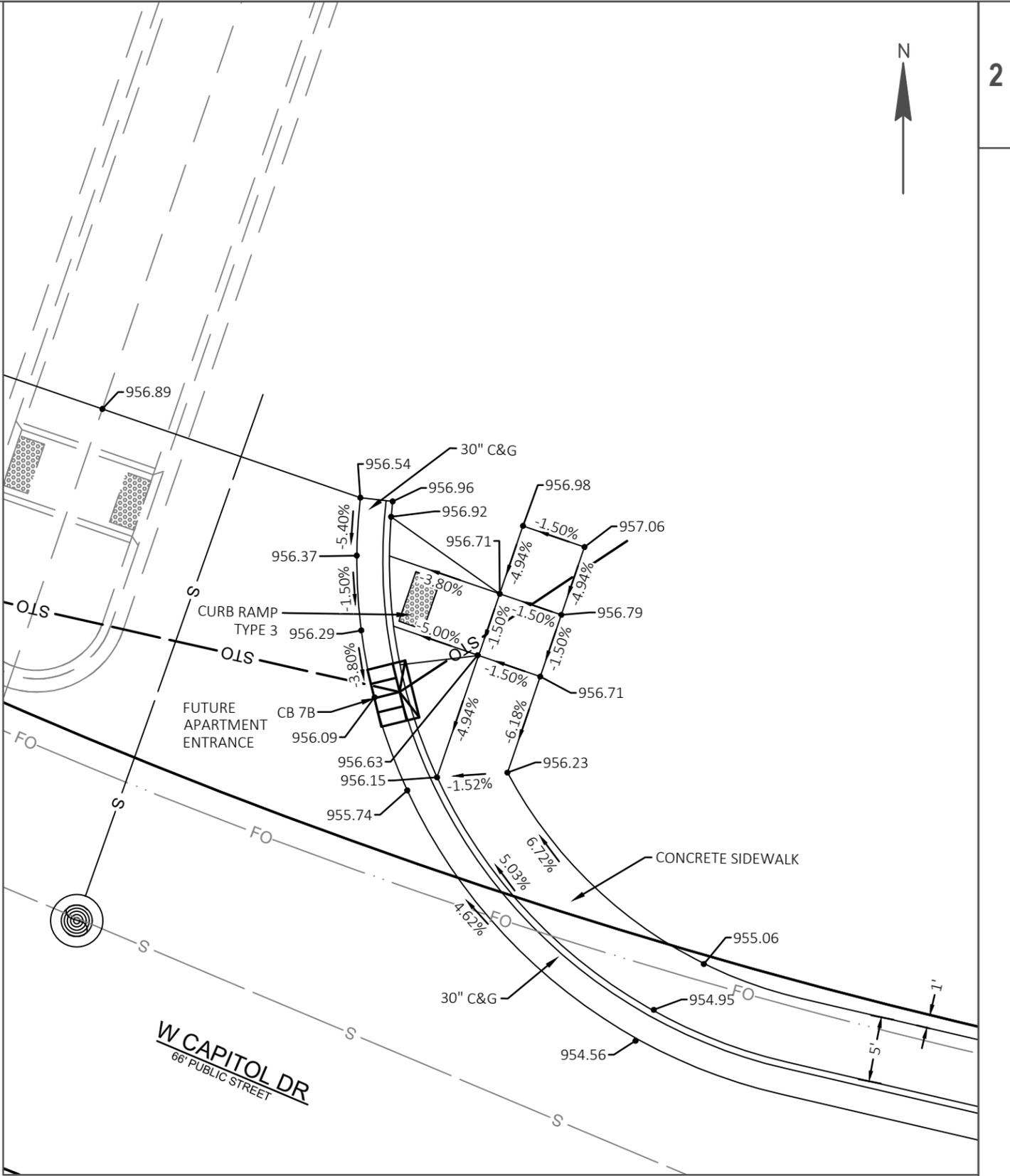
W. CAPITOL DRIVE
CURB RAMP
STA: 28+74.69, 31' RT



W. CAPITOL DRIVE
CURB RAMP
STA: 30+82.50, 22' RT



W. CAPITOL DRIVE
CURB RAMP
STA: 31+15.84, 46.5' LT



W. CAPITOL DRIVE
CURB RAMP
STA: 31+83.65, 52' LT

PROJECT NO: 490823	HWY: STH 83	COUNTY: WAUKESHA	INTERSECTION RAMP DETAILS	SHEET 25	E
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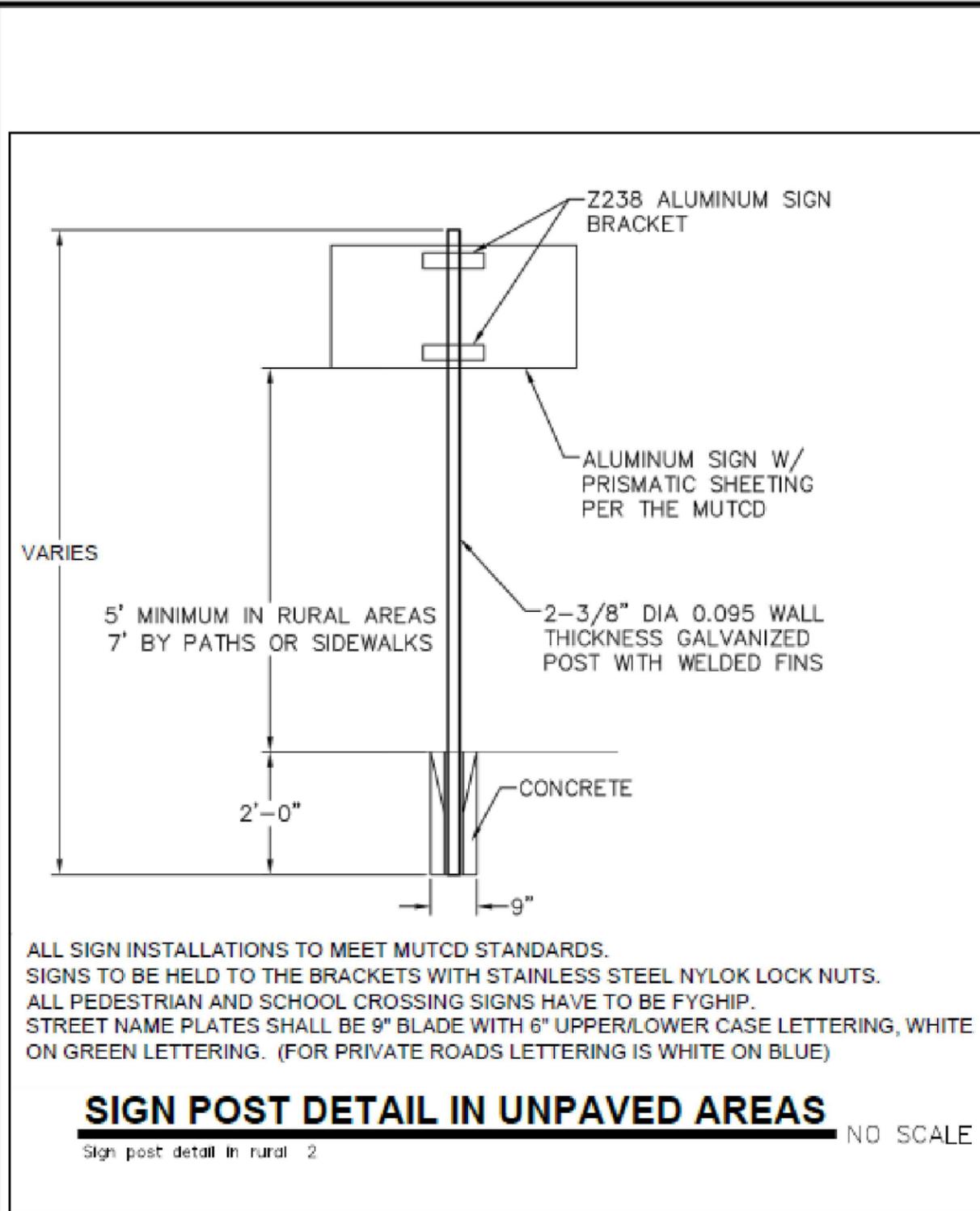


EXHIBIT GN-02

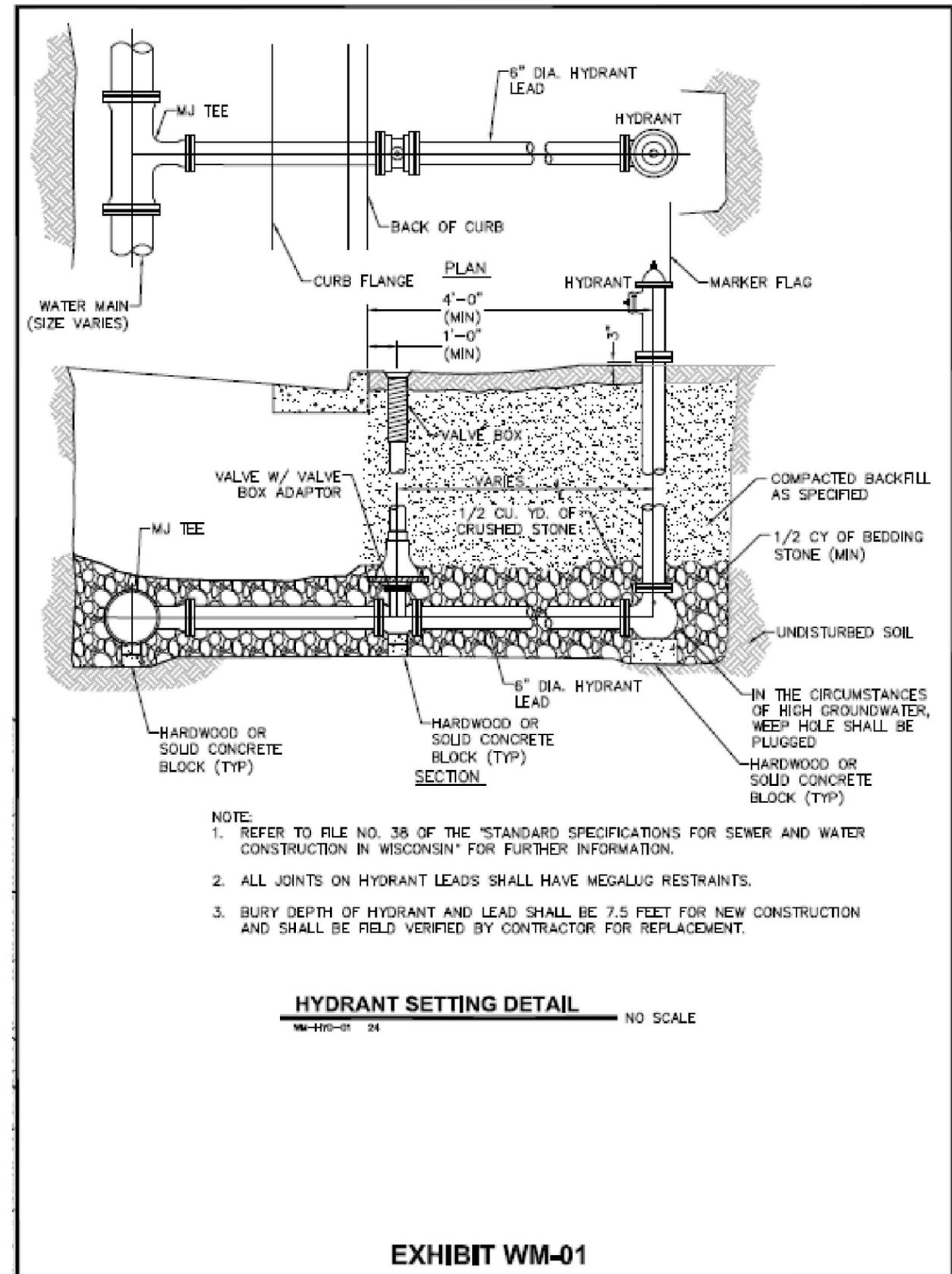
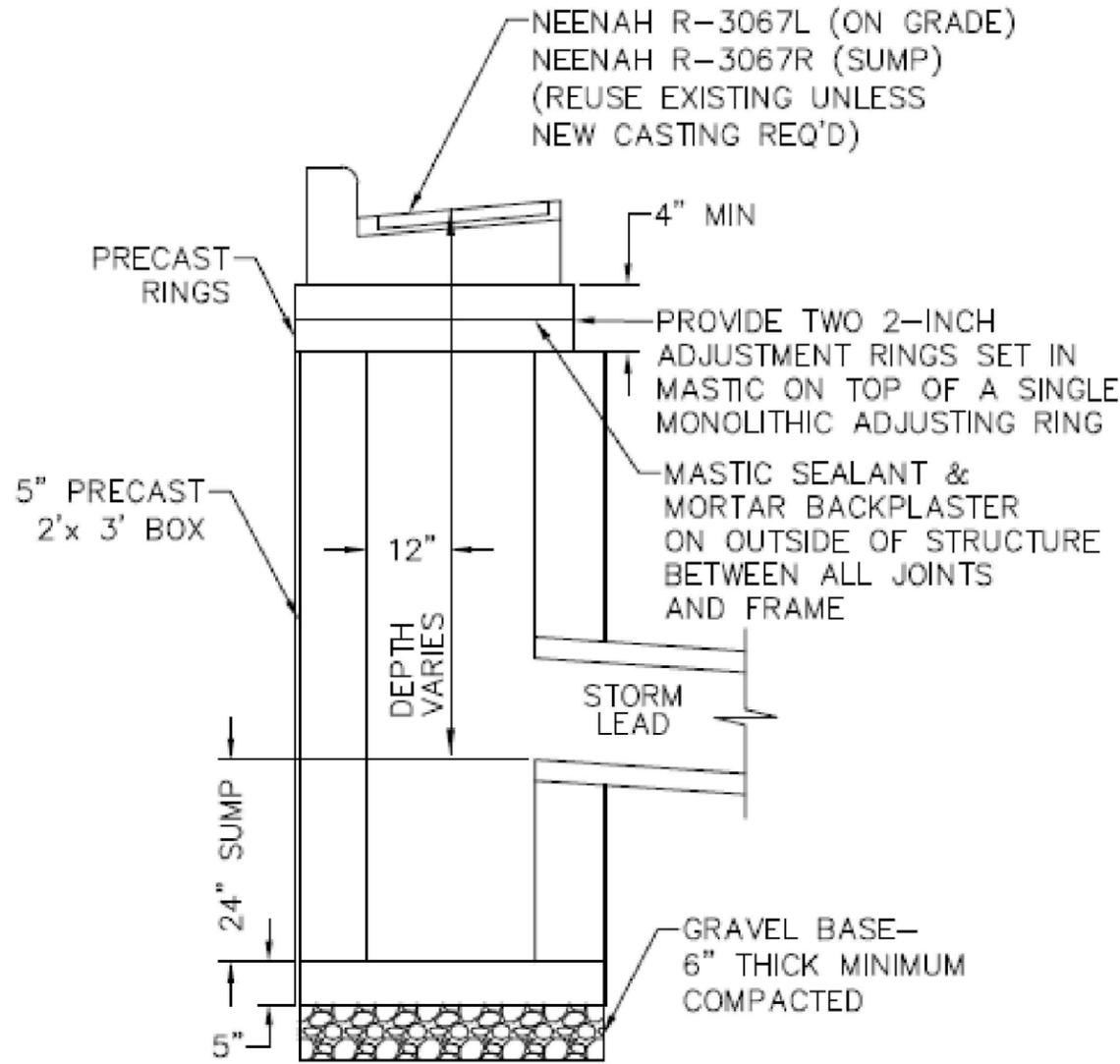


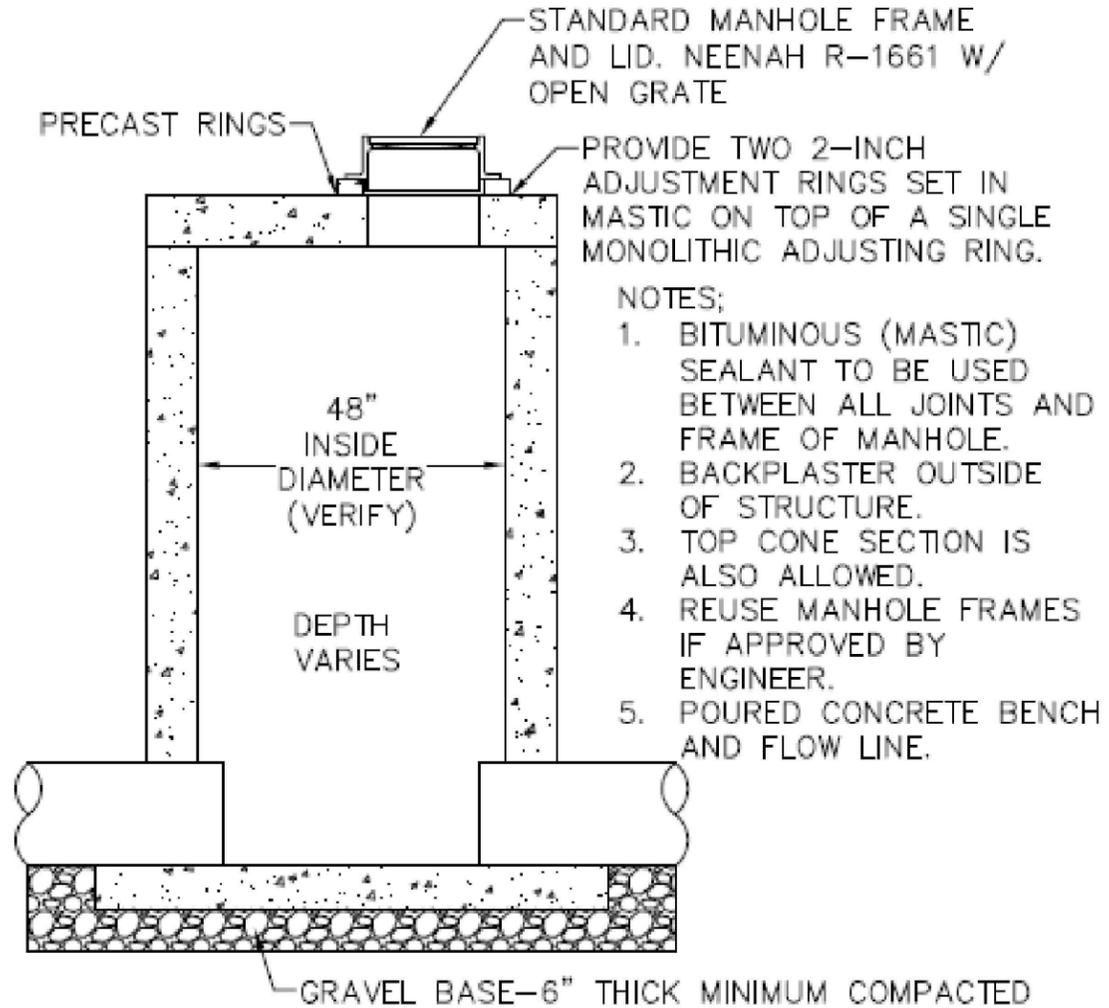
EXHIBIT WM-01



1. INLET TO BE PROTECTED WITH GEOTEXTILE FABRIC BETWEEN THE GRATE AND FRAME. PROTECTION TO REMAIN IN PLACE UNTIL SITE VEGETATION IS ESTABLISHED.
2. IN AREAS WITH ROLL FACE CURB, REPLACEMENT CURB SHALL TRANSITION TO VERTICAL FACE CURB & GUTTER AT INLET FRAME.

CATCH BASIN DETAIL
STO-03 20 NO SCALE

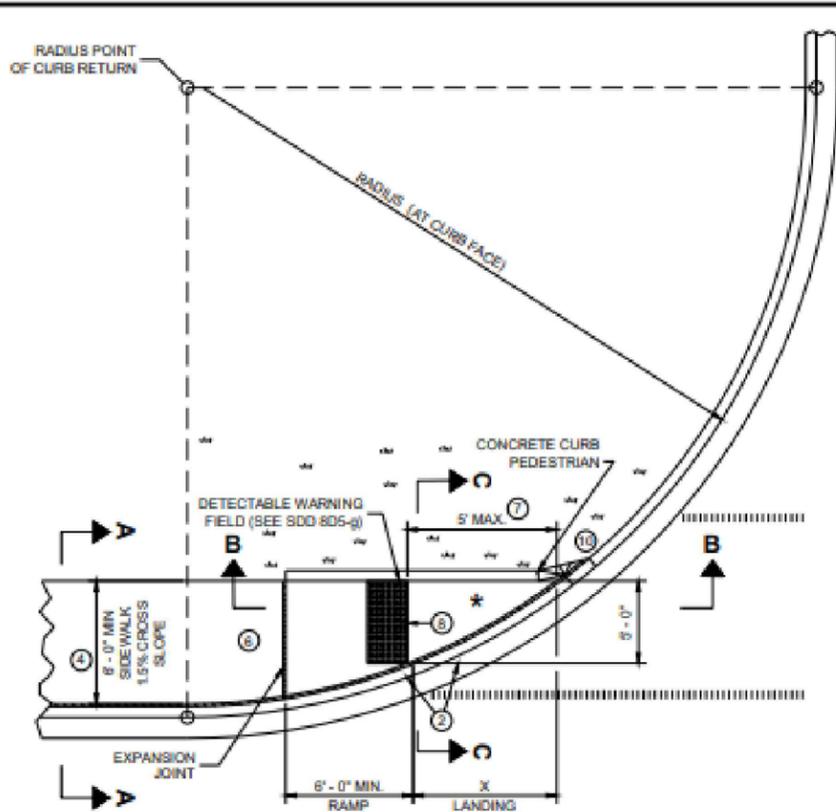
EXHIBIT STO-03



- NOTES:
1. BITUMINOUS (MASTIC) SEALANT TO BE USED BETWEEN ALL JOINTS AND FRAME OF MANHOLE.
 2. BACKPLASTER OUTSIDE OF STRUCTURE.
 3. TOP CONE SECTION IS ALSO ALLOWED.
 4. REUSE MANHOLE FRAMES IF APPROVED BY ENGINEER.
 5. POURED CONCRETE BENCH AND FLOW LINE.

STORM MANHOLE DETAIL
STO-01 20 NO SCALE

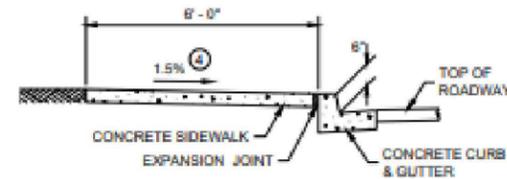
EXHIBIT STO-01



**PLAN VIEW
CURB RAMP TYPE 4A**

RADIUS (AT CURB FACE)	X
10 FEET	4' - 7"

INTERMEDIATE RADII CAN BE INTERPOLATED



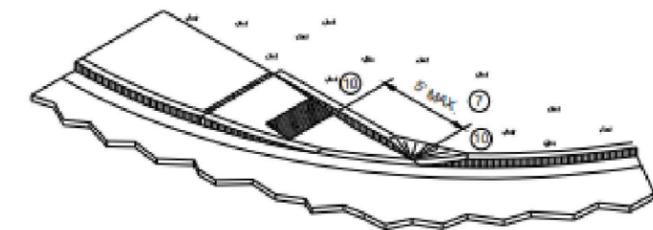
SECTION A - A FOR TYPE 4A

GENERAL NOTES

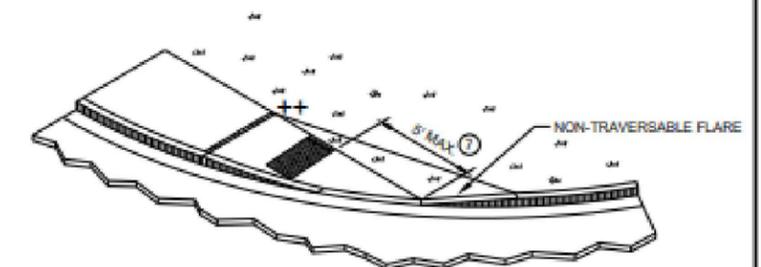
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/8" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-I.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

LEGEND

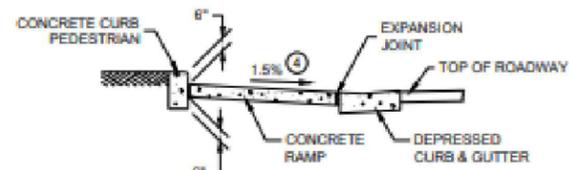
- 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)



ISOMETRIC VIEW FOR TYPE 4A

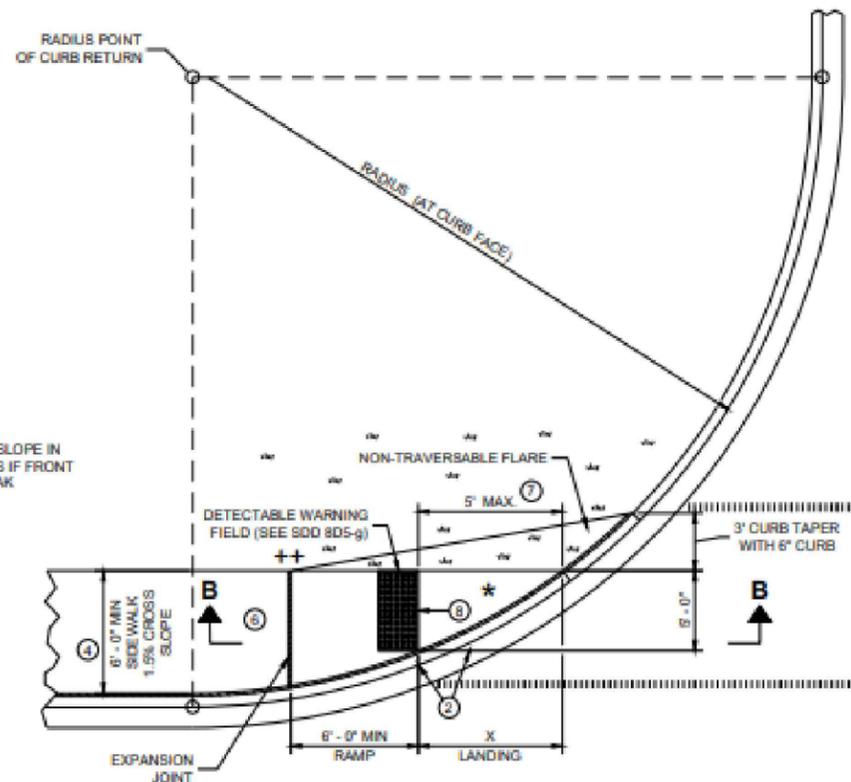


ISOMETRIC VIEW FOR TYPE 4A1



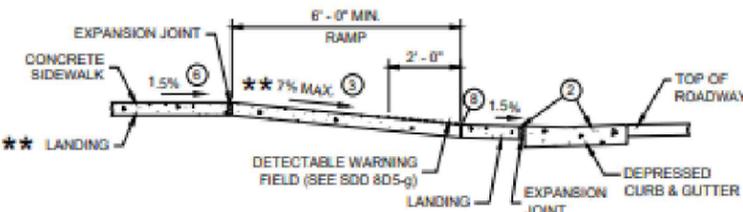
SECTION C - C FOR TYPE 4A

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK



**PLAN VIEW
CURB RAMP TYPE 4A1**

++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE



**SECTION B - B FOR
TYPE 4A AND TYPE 4A1**

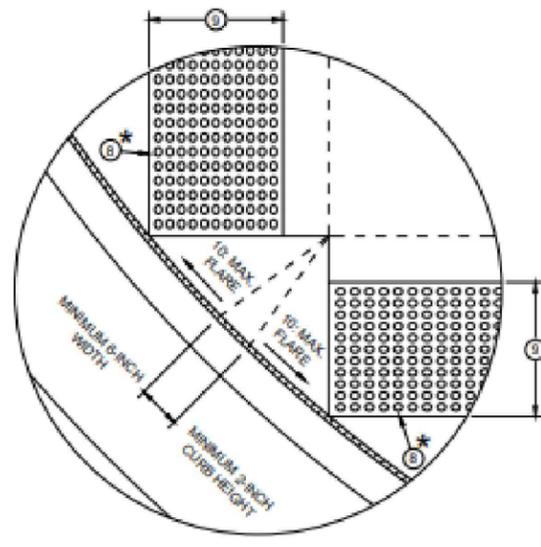
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

**CURB RAMPS
TYPE 4A AND 4A1**

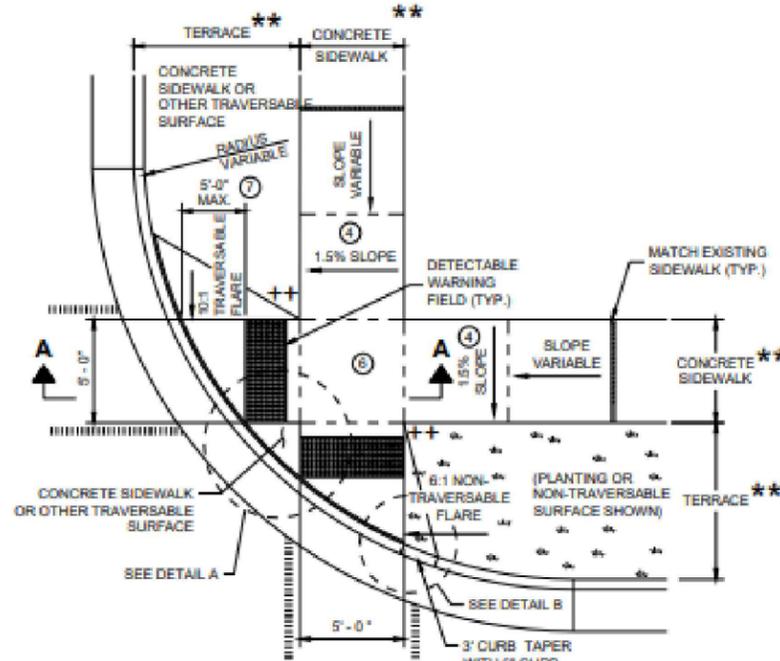
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

SDD 08D05-21C

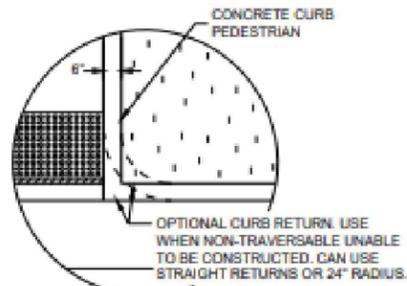
SDD 08D05-21C



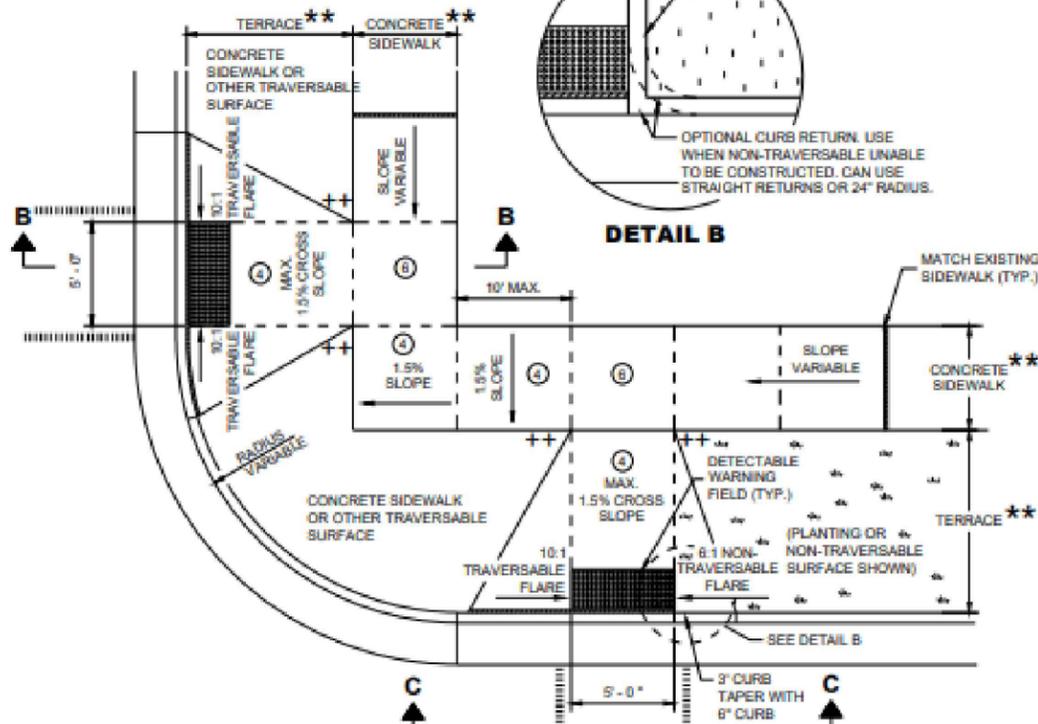
DETAIL A



PLAN VIEW CURB RAMP TYPE 2 (CENTER OF CORNER RADIUS)



DETAIL B



PLAN VIEW CURB RAMP TYPE 3 (OUTSIDE OF CROSSWALK AREA)

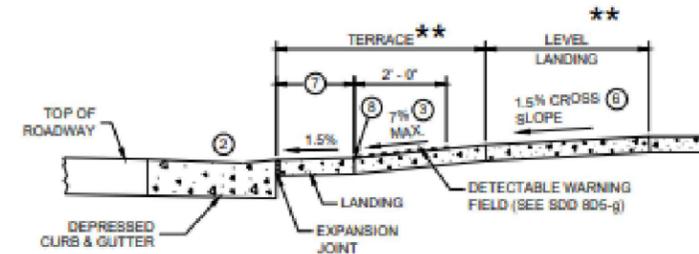
GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.

- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/8" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.
- ⑦ WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑨ WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.



SECTION A - A FOR TYPE 2

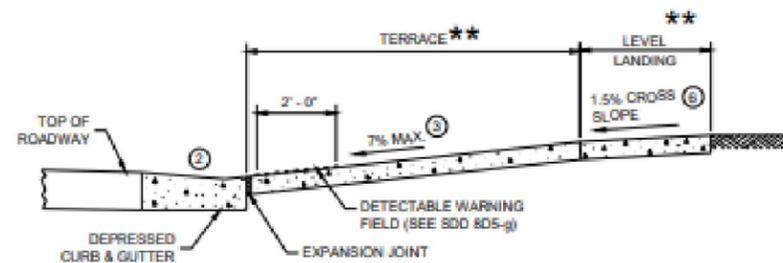
* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

** WIDTH SHOWN ELSEWHERE IN THE PLANS

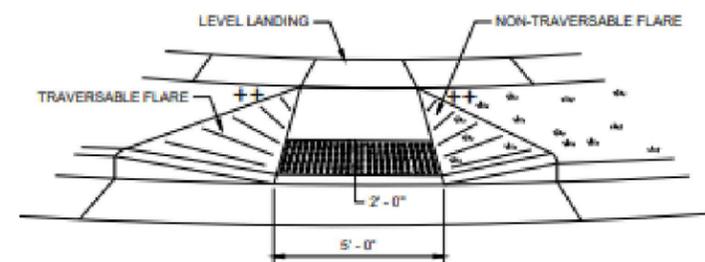
++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - CONTRACTION JOINT SIDEWALK
- |||||| PAVEMENT MARKING CROSSWALK (WHITE)



SECTION B - B FOR TYPE 3



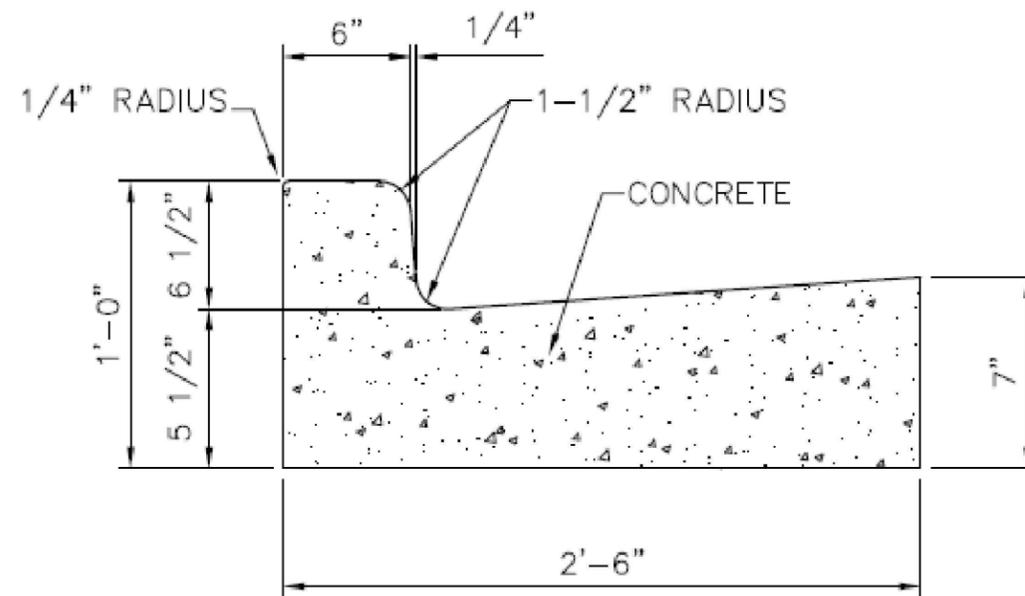
VIEW C - C FOR TYPE 3

CURB RAMPS TYPE 2 AND 3

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

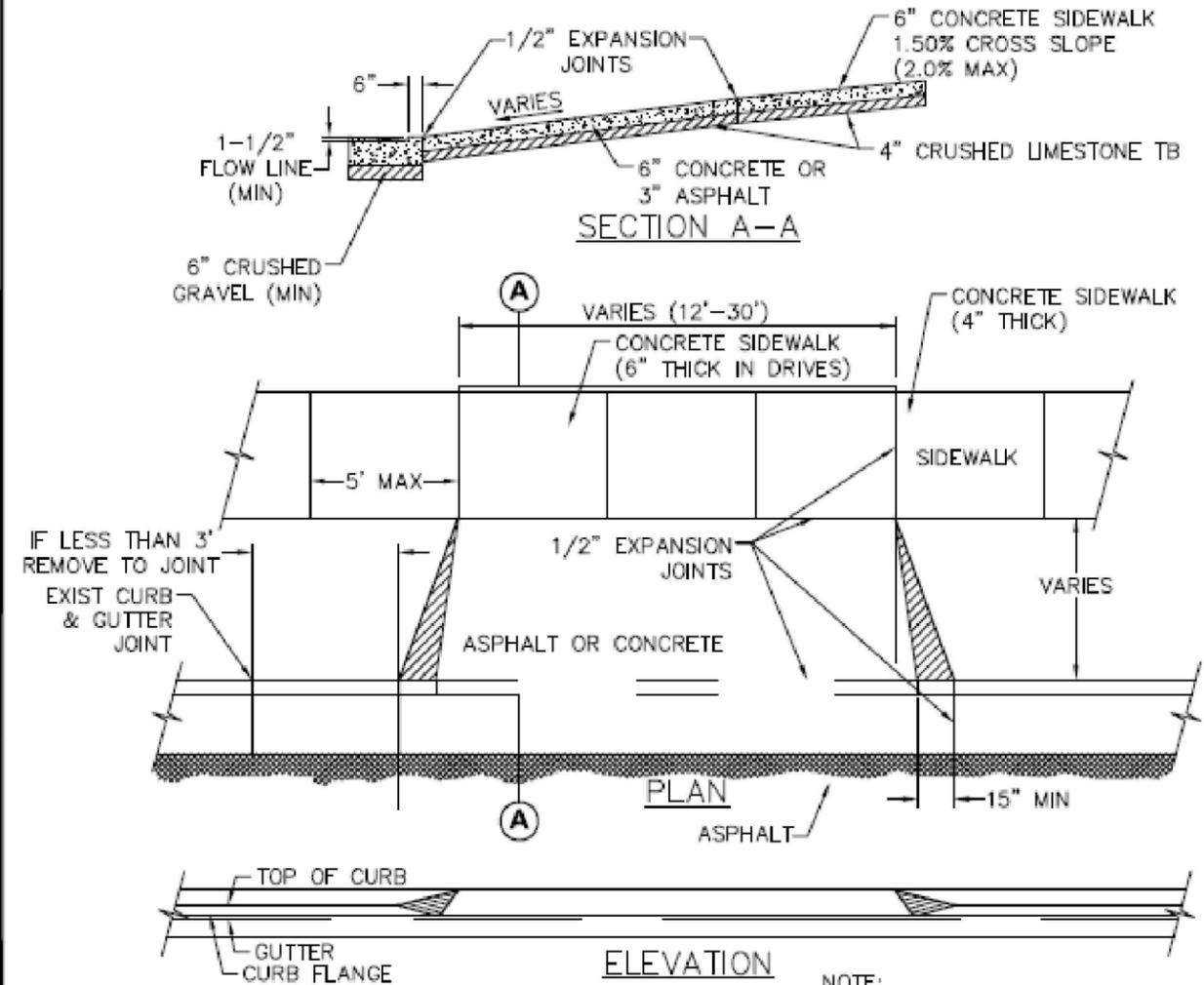
SDD 08D05-21b

SDD 08D05-21b



30" VERTICAL FACE CURB & GUTTER
 NO SCALE
 02770C&G2 1

EXHIBIT PV-04

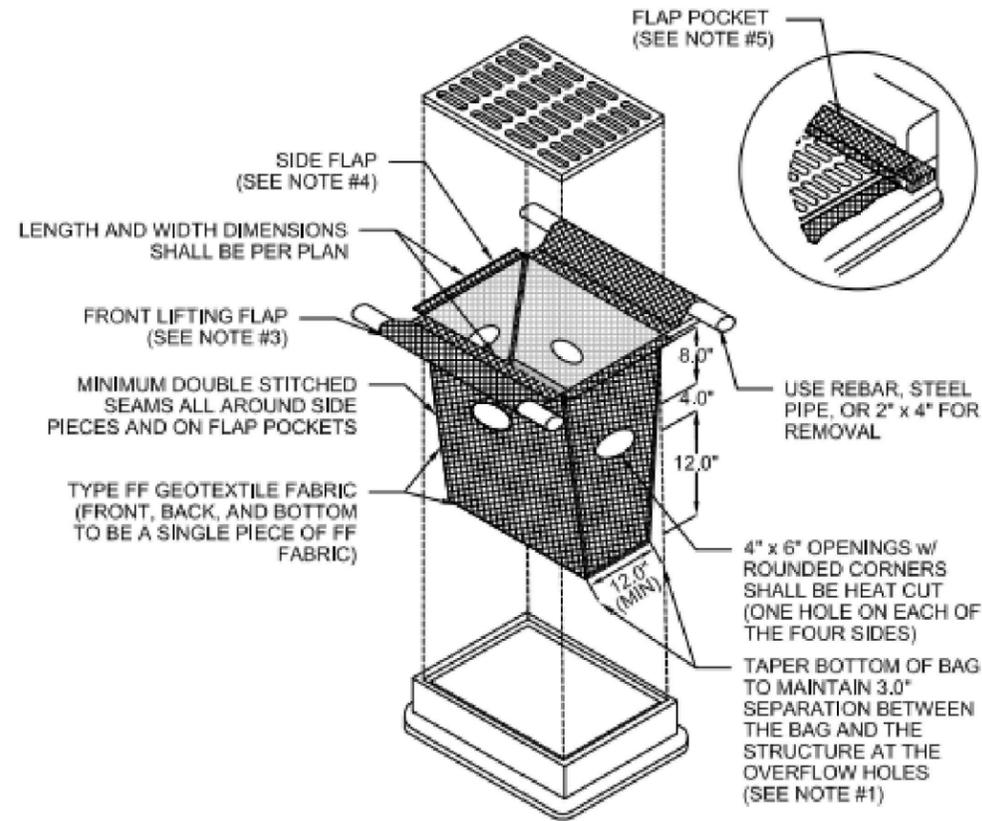


ASPHALT OR CONCRETE DRIVEWAY
 NO SCALE
 drivecpr 48

- NOTE:
1. IN AREAS WHERE CURB IS MOUNTABLE, DRIVEWAYS TO MATCH TOP OF CURB.
 2. DRIVEWAY OPENING AT CURB MINIMUM 15' AND MAXIMUM 33'.

EXHIBIT PV-02

FIGURE 2. INLET PROTECTION TYPE D



INLET PROTECTION TYPE D

CAN BE INSTALLED IN INLETS WITH OR WITHOUT CURB BOXES

NOTES:

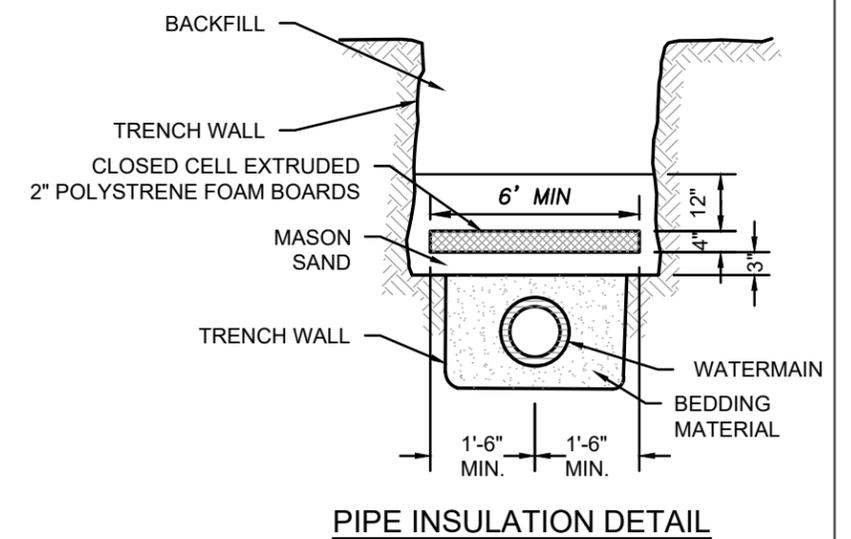
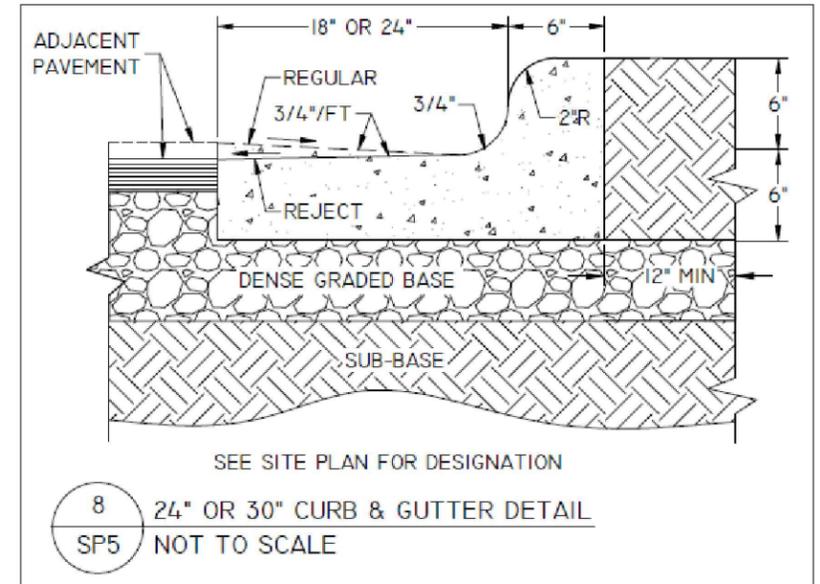
1. TAPER BOTTOM OF BAG TO MAINTAIN 3" OF CLEARANCE BETWEEN THE BAG AND THE STRUCTURE, MEASURED FROM THE BOTTOM OF THE OVERFLOW OPENINGS TO THE STRUCTURE WALL.
2. GEOTEXTILE FABRIC TYPE FF FOR FLAPS, TOP AND BOTTOM OF OUTSIDE OF FILTER BAG. FRONT, BACK, AND BOTTOM OF FILTER BAG BEING ONE PIECE.
3. FRONT LIFTING FLAP IS TO BE USED WHEN REMOVING AND MAINTAINING FILTER BAG.
4. SIDE FLAPS SHALL BE A MAXIMUM OF 2" LONG. FOLD THE FABRIC OVER AND REINFORCE WITH MULTIPLE STITCHES.
5. FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2" x 4". THE REBAR, STEEL PIPE, OR WOOD SHALL BE INSTALLED IN THE REAR FLAP AND SHALL NOT BLOCK THE TOP HALF OF THE CURB FACE OPENING.

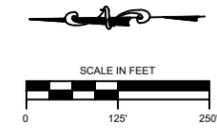
MAINTENANCE NOTES:

1. WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED IN THE FABRIC DOES NOT FALL INTO THE STRUCTURE. MATERIAL THAT HAS FALLEN INTO THE INLET SHALL BE IMMEDIATELY REMOVED.



1060
TECHNICAL STANDARD No.
12/2021
REVISION DATE
NOT TO SCALE





TRAFFIC CONTROL NOTES

1. HOURS OF CONSTRUCTION OPERATIONS SHALL BE FROM 6AM TO 6PM DAILY.
2. ALL UTILITY TRENCHES SHALL BE BACKFILLED AND STRUCTURAL PLATED AT THE END OF EACH WORK DAY.
3. TRAFFIC CONTROL DRUMS SHALL BE PLACED AT 25-FT SPACING (MAX.) ADJACENT TO LIVE TRAFFIC LANES. DRUMS TO REMAIN IN PLACE DURING NIGHT HOURS SHALL INCLUDE TYPE C LIGHTS PER WisDOT S.D.D. 15-
4. ADVANCED WARNING SIGNS FOR WORK ZONE SHALL FOLLOW WisDOT S.D.D. 15C5-05 FOR 40 M.P.H. OR LESS, TWO WAY UNDIVIDED ROAD OPEN TRAFFIC.
5. FLAGGING OPERATIONS FOR UTILITY TRENCH WORK SHALL FOLLOW WisDOT S.D.D. 15C12-a TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION.
6. TRAFFIC CONTROL SINGLE LANE CLOSURE SHALL FOLLOW WisDOT S.D.D. 15D12a.
7. TRAFFIC CONTROL SIGNING ON ROADWAY WITH LOOSE GRAVEL SHALL FOLLOW WisDOT 15D45-03.
8. TRAFFIC CONTROL SIGNING ON ROADWAY WITH MILLED SURFACE SHALL FOLLOW WisDOT 15D44-02.
9. ALL ROADWAYS SHALL HAVE ONE LANE OPEN IN EACH DIRECTION DURING NON-CONSTRUCTION WORK HOURS WITH MINIMUM 11-FT WIDTH.

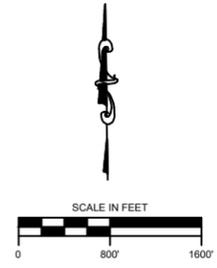
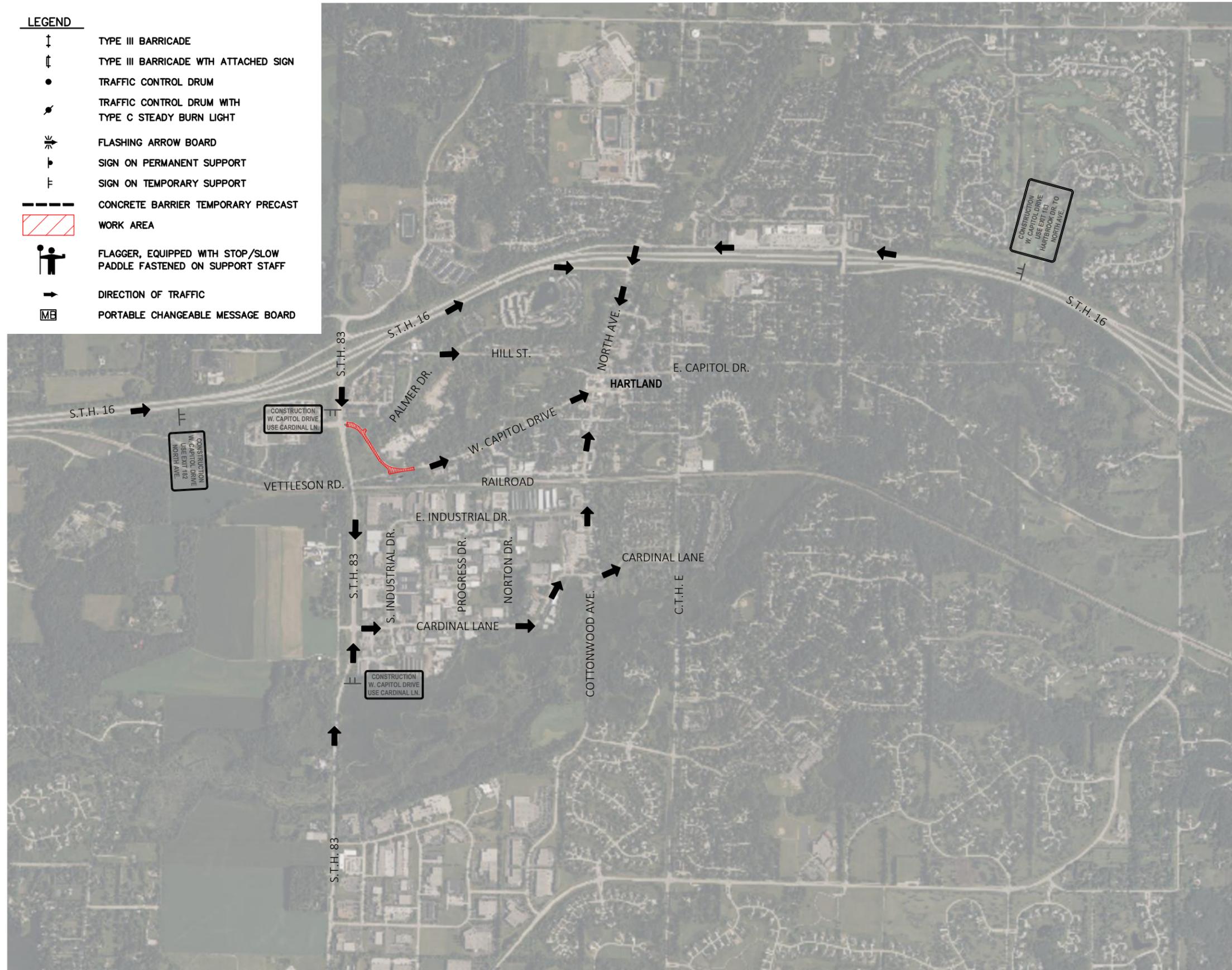
LEGEND

- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- FLASHING ARROW BOARD
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- CONCRETE BARRIER TEMPORARY PRECAST
- WORK AREA
- FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
- DIRECTION OF TRAFFIC
- PORTABLE CHANGEABLE MESSAGE BOARD

THE INFORMATION SHOWN ON THIS DRAWING CONCERNING TYPE AND LOCATION OF UNDERGROUND UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE FOR MAKING HIS OWN DETERMINATIONS AS TO THE TYPE AND LOCATION OF UNDERGROUND UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. IF ADDITIONAL UTILITIES ARE KNOWN TO EXIST IN THE PROPERTY, THE OWNER WILL PROVIDE EXISTING PLANS OF OTHER UTILITIES SERVING THE SITE AND THE BUILDING THAT OTHERWISE CANNOT BE LOCATED BY A VISUAL OBSERVATION OF THE PROPERTY OR OF WHICH THE SURVEYOR WOULD HAVE NO KNOWLEDGE.

DIGGERS HOTLINE
 Toll Free (800) 242-8511
 Milwaukee Area (414) 259-1181
 Hearing Impaired TDD (800) 542-2289
 www.DiggersHotline.com

- LEGEND**
- ↓ TYPE III BARRICADE
 - ↓ TYPE III BARRICADE WITH ATTACHED SIGN
 - TRAFFIC CONTROL DRUM
 - TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
 - ↔ FLASHING ARROW BOARD
 - ⊥ SIGN ON PERMANENT SUPPORT
 - ⊥ SIGN ON TEMPORARY SUPPORT
 - CONCRETE BARRIER TEMPORARY PRECAST
 - ▨ WORK AREA
 - ⚠ FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
 - DIRECTION OF TRAFFIC
 - MB PORTABLE CHANGEABLE MESSAGE BOARD



TRAFFIC CONTROL NOTES

1. HOURS OF CONSTRUCTION OPERATIONS SHALL BE FROM 6AM TO 6PM DAILY.
2. ALL UTILITY TRENCHES SHALL BE BACKFILLED AND STRUCTURAL PLATED AT THE END OF EACH WORK DAY.
3. TRAFFIC CONTROL DRUMS SHALL BE PLACED AT 25-FT SPACING (MAX.) ADJACENT TO LIVE TRAFFIC LANES. DRUMS TO REMAIN IN PLACE DURING NIGHT HOURS SHALL INCLUDE TYPE C LIGHTS PER WisDOT S.D.D. 15-
4. ADVANCED WARNING SIGNS FOR WORK ZONE SHALL FOLLOW WisDOT S.D.D. 15C5-05 FOR 40 M.P.H. OR LESS, TWO WAY UNDIVIDED ROAD OPEN TRAFFIC.
5. FLAGGING OPERATIONS FOR UTILITY TRENCH WORK SHALL FOLLOW WisDOT S.D.D. 15C12-a TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION.
6. TRAFFIC CONTROL SINGLE LANE CLOSURE SHALL FOLLOW WisDOT S.D.D. 15D12a.
7. TRAFFIC CONTROL SIGNING ON ROADWAY WITH LOOSE GRAVEL SHALL FOLLOW WisDOT 15D45-03.
8. TRAFFIC CONTROL SIGNING ON ROADWAY WITH MILLED SURFACE SHALL FOLLOW WisDOT 15D44-02.
9. ALL ROADWAYS SHALL HAVE ONE LANE OPEN IN EACH DIRECTION DURING NON-CONSTRUCTION WORK HOURS WITH MINIMUM 11-FT WIDTH.

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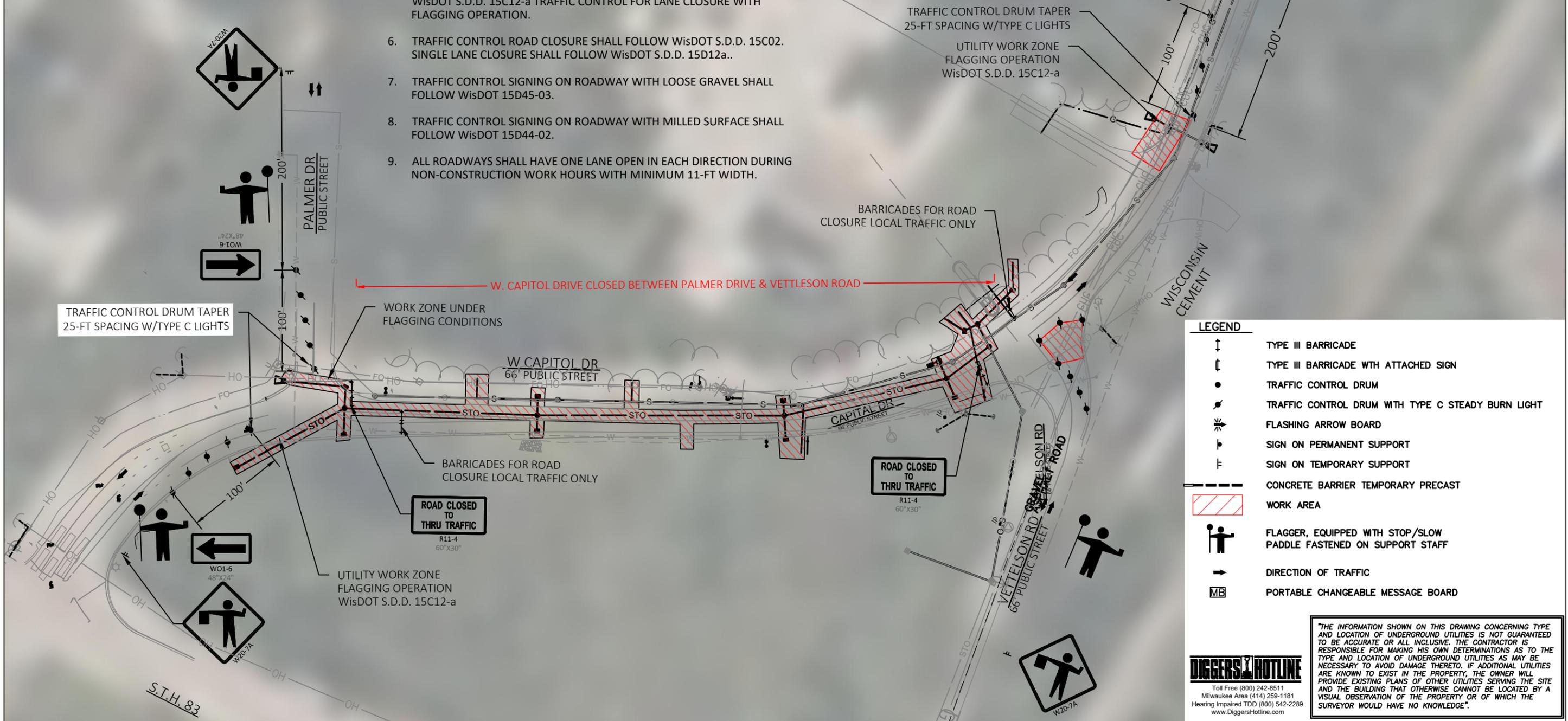
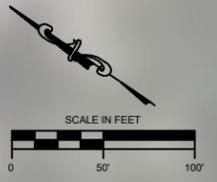
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CONSTRUCTION - PHASE 1

1. INSTALL ADVANCED WARNING TRAFFIC CONTROL SIGNAGE FOR FLAGGING OPERATION ON W. CAPITOL DRIVE, PALMER DRIVE, AND VETTELSON ROAD.
2. INSTALL ROAD CLOSURE BETWEEN PALMER DRIVE AND VETTELSON ROAD.
3. COMMENCE UTILITY INSTALLATION (SEWER, WATER STORM AND SERVICE LATERALS). COMPLETE REMOVALS AND FINE GRADE. ROADWAY SHALL HAVE MINIMUM 11-FT LANES IN EACH DIRECTION WHEN OPEN TO TRAFFIC.
4. UTILITY TRENCHES TO BE PLATED OR GRAVELED AT END OF EACH WORKDAY.
5. WISCONSIN CEMENT HAS 5-6 SEMI TRUCK DELIVERIES PER WEEK THAT WILL NEED TO BE ACCOMMODATED AND REQUIRE ACCESS TO S.T.H. 83 AND W. CAPITOL DRIVE.

TRAFFIC CONTROL NOTES

1. HOURS OF CONSTRUCTION OPERATIONS SHALL BE FROM 6AM TO 6PM DAILY.
2. ALL UTILITY TRENCHES SHALL BE BACKFILLED AND STRUCTURAL PLATED AT THE END OF EACH WORK DAY.
3. TRAFFIC CONTROL DRUMS SHALL BE PLACED AT 25-FT SPACING (MAX.) ADJACENT TO LIVE TRAFFIC LANES. DRUMS TO REMAIN IN PLACE DURING NIGHT HOURS SHALL INCLUDE TYPE C LIGHTS PER WisDOT S.D.D. 15-
4. ADVANCED WARNING SIGNS FOR WORK ZONE SHALL FOLLOW WisDOT S.D.D. 15C5-05 FOR 40 M.P.H. OR LESS, TWO WAY UNDIVIDED ROAD OPEN TRAFFIC.
5. FLAGGING OPERATIONS FOR UTILITY TRENCH WORK SHALL FOLLOW WisDOT S.D.D. 15C12-a TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION.
6. TRAFFIC CONTROL ROAD CLOSURE SHALL FOLLOW WisDOT S.D.D. 15C02. SINGLE LANE CLOSURE SHALL FOLLOW WisDOT S.D.D. 15D12a..
7. TRAFFIC CONTROL SIGNING ON ROADWAY WITH LOOSE GRAVEL SHALL FOLLOW WisDOT 15D45-03.
8. TRAFFIC CONTROL SIGNING ON ROADWAY WITH MILLED SURFACE SHALL FOLLOW WisDOT 15D44-02.
9. ALL ROADWAYS SHALL HAVE ONE LANE OPEN IN EACH DIRECTION DURING NON-CONSTRUCTION WORK HOURS WITH MINIMUM 11-FT WIDTH.



LEGEND

	TYPE III BARRICADE
	TYPE III BARRICADE WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	FLASHING ARROW BOARD
	SIGN ON PERMANENT SUPPORT
	SIGN ON TEMPORARY SUPPORT
	CONCRETE BARRIER TEMPORARY PRECAST
	WORK AREA
	FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
	DIRECTION OF TRAFFIC
	PORTABLE CHANGEABLE MESSAGE BOARD

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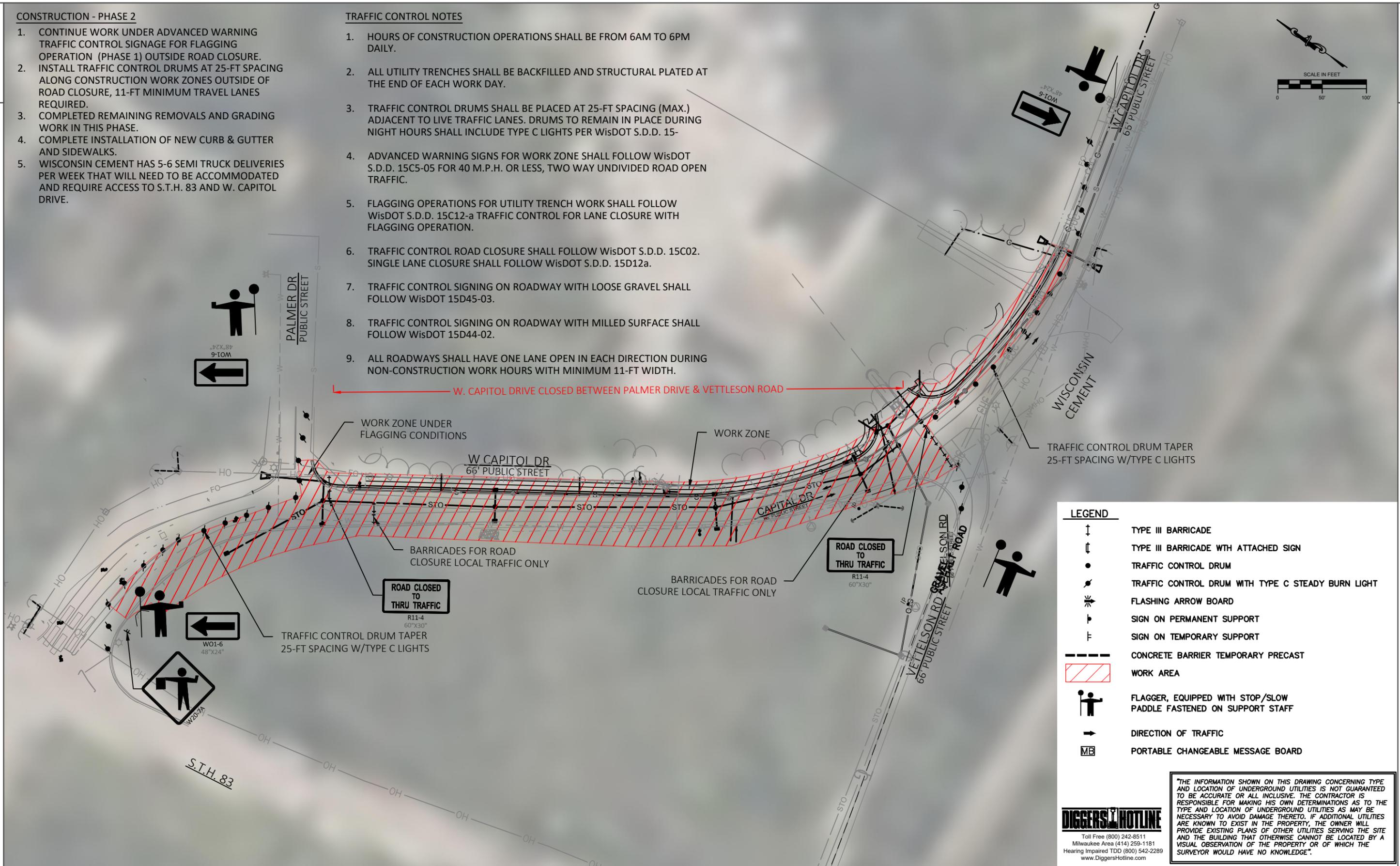
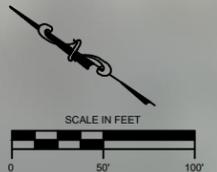
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CONSTRUCTION - PHASE 2

1. CONTINUE WORK UNDER ADVANCED WARNING TRAFFIC CONTROL SIGNAGE FOR FLAGGING OPERATION (PHASE 1) OUTSIDE ROAD CLOSURE.
2. INSTALL TRAFFIC CONTROL DRUMS AT 25-FT SPACING ALONG CONSTRUCTION WORK ZONES OUTSIDE OF ROAD CLOSURE, 11-FT MINIMUM TRAVEL LANES REQUIRED.
3. COMPLETED REMAINING REMOVALS AND GRADING WORK IN THIS PHASE.
4. COMPLETE INSTALLATION OF NEW CURB & GUTTER AND SIDEWALKS.
5. WISCONSIN CEMENT HAS 5-6 SEMI TRUCK DELIVERIES PER WEEK THAT WILL NEED TO BE ACCOMMODATED AND REQUIRE ACCESS TO S.T.H. 83 AND W. CAPITOL DRIVE.

TRAFFIC CONTROL NOTES

1. HOURS OF CONSTRUCTION OPERATIONS SHALL BE FROM 6AM TO 6PM DAILY.
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5. FLAGGING OPERATIONS FOR UTILITY TRENCH WORK SHALL FOLLOW WisDOT S.D.D. 15C12-a TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION.
6. TRAFFIC CONTROL ROAD CLOSURE SHALL FOLLOW WisDOT S.D.D. 15C02. SINGLE LANE CLOSURE SHALL FOLLOW WisDOT S.D.D. 15D12a.
7. TRAFFIC CONTROL SIGNING ON ROADWAY WITH LOOSE GRAVEL SHALL FOLLOW WisDOT 15D45-03.
8. TRAFFIC CONTROL SIGNING ON ROADWAY WITH MILLED SURFACE SHALL FOLLOW WisDOT 15D44-02.
9. ALL ROADWAYS SHALL HAVE ONE LANE OPEN IN EACH DIRECTION DURING NON-CONSTRUCTION WORK HOURS WITH MINIMUM 11-FT WIDTH.



LEGEND

↑	TYPE III BARRICADE
↑↓	TYPE III BARRICADE WITH ATTACHED SIGN
●	TRAFFIC CONTROL DRUM
●	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
↔	FLASHING ARROW BOARD
↑	SIGN ON PERMANENT SUPPORT
↑	SIGN ON TEMPORARY SUPPORT
---	CONCRETE BARRIER TEMPORARY PRECAST
▨	WORK AREA
⚠	FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
→	DIRECTION OF TRAFFIC
MB	PORTABLE CHANGEABLE MESSAGE BOARD

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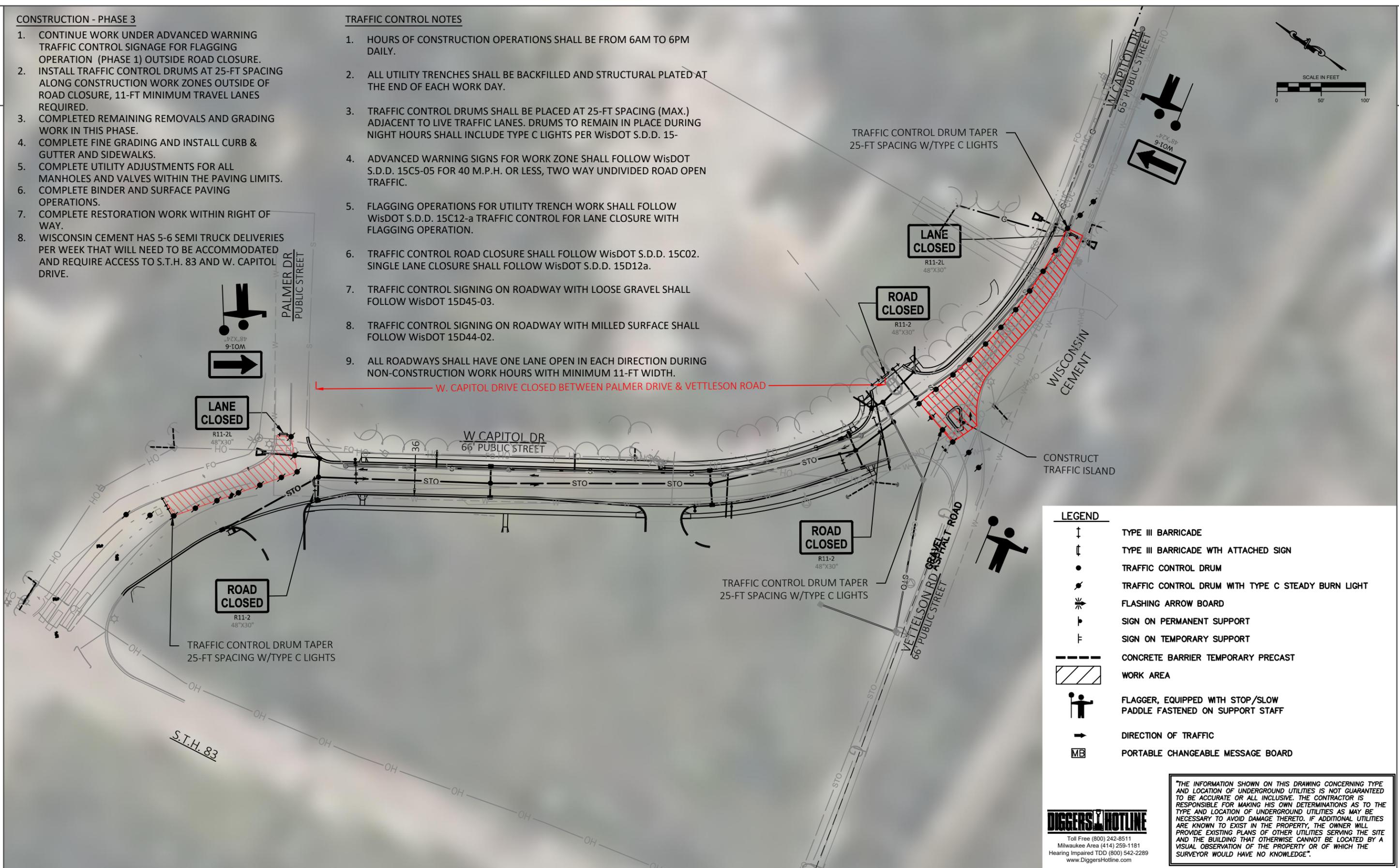
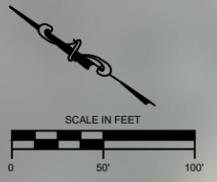
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CONSTRUCTION - PHASE 3

1. CONTINUE WORK UNDER ADVANCED WARNING TRAFFIC CONTROL SIGNAGE FOR FLAGGING OPERATION (PHASE 1) OUTSIDE ROAD CLOSURE.
2. INSTALL TRAFFIC CONTROL DRUMS AT 25-FT SPACING ALONG CONSTRUCTION WORK ZONES OUTSIDE OF ROAD CLOSURE, 11-FT MINIMUM TRAVEL LANES REQUIRED.
3. COMPLETED REMAINING REMOVALS AND GRADING WORK IN THIS PHASE.
4. COMPLETE FINE GRADING AND INSTALL CURB & GUTTER AND SIDEWALKS.
5. COMPLETE UTILITY ADJUSTMENTS FOR ALL MANHOLES AND VALVES WITHIN THE PAVING LIMITS.
6. COMPLETE BINDER AND SURFACE PAVING OPERATIONS.
7. COMPLETE RESTORATION WORK WITHIN RIGHT OF WAY.
8. WISCONSIN CEMENT HAS 5-6 SEMI TRUCK DELIVERIES PER WEEK THAT WILL NEED TO BE ACCOMMODATED AND REQUIRE ACCESS TO S.T.H. 83 AND W. CAPITOL DRIVE.

TRAFFIC CONTROL NOTES

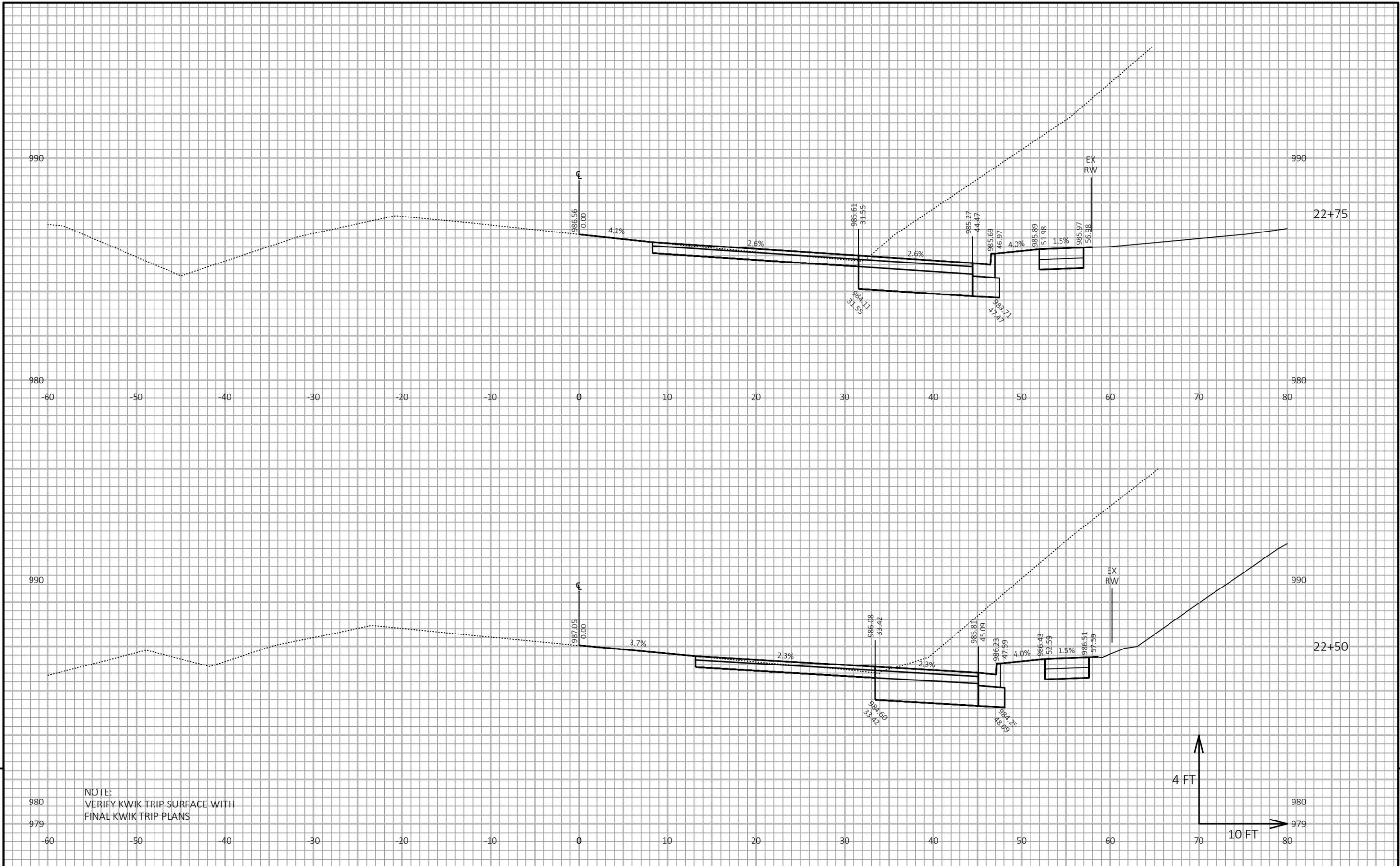
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6. TRAFFIC CONTROL ROAD CLOSURE SHALL FOLLOW WisDOT S.D.D. 15C02. SINGLE LANE CLOSURE SHALL FOLLOW WisDOT S.D.D. 15D12a.
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	TYPE III BARRICADE
	TYPE III BARRICADE WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
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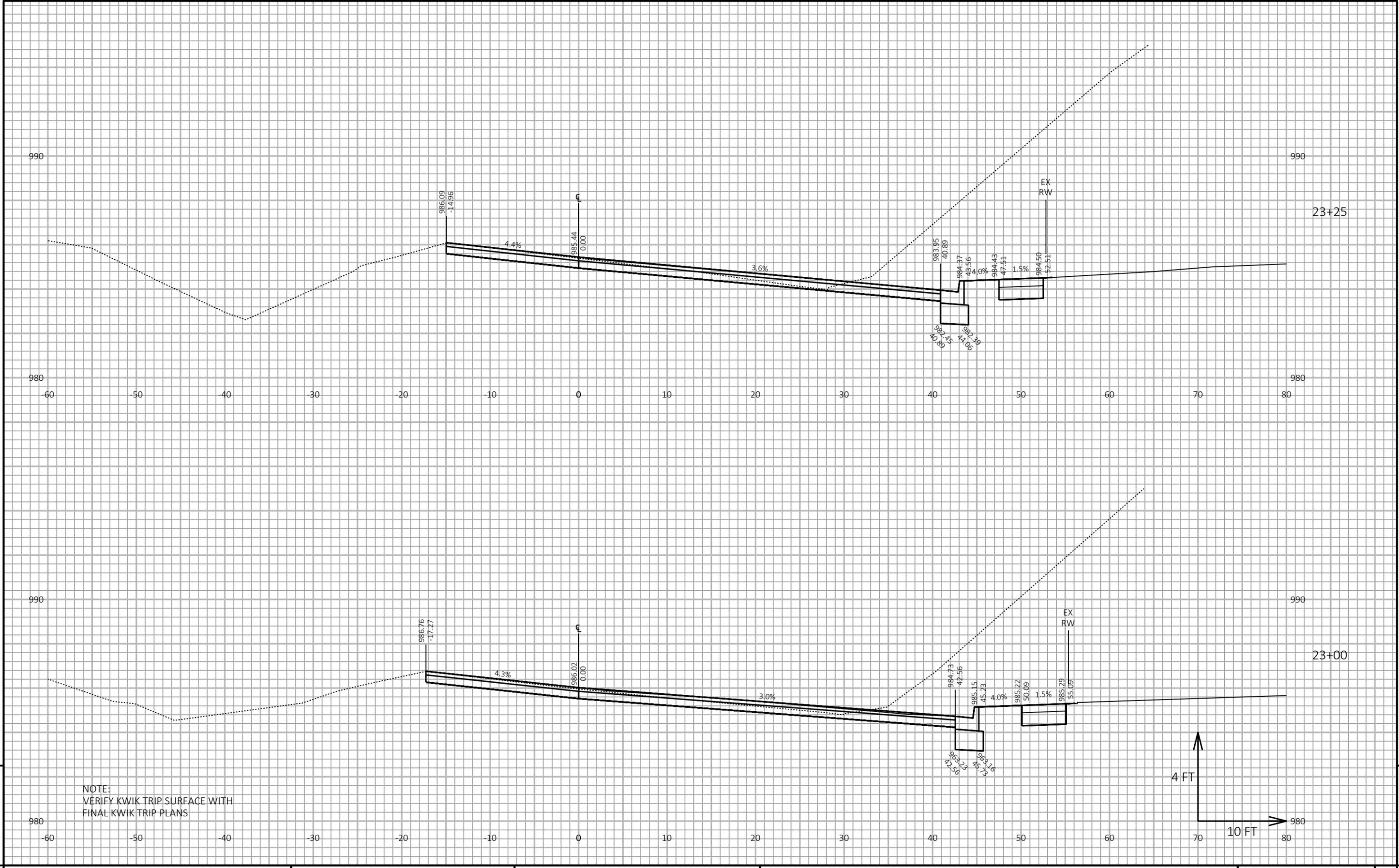
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COUNTY: WAUKESHA

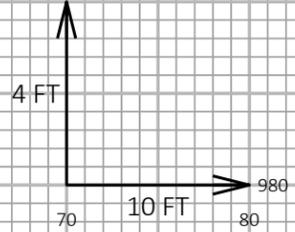
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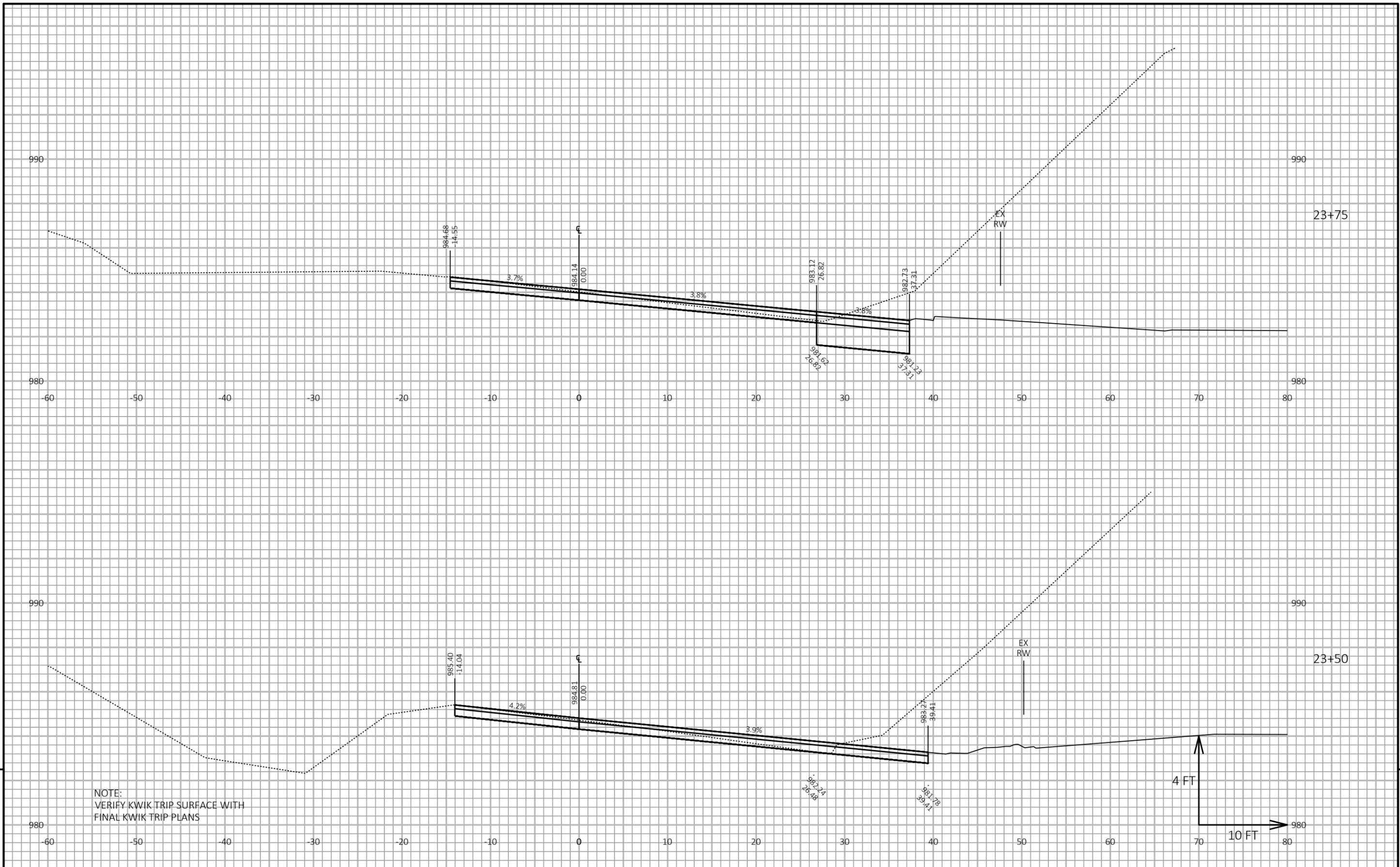


NOTE:
VERIFY KWIK TRIP SURFACE WITH
FINAL KWIK TRIP PLANS



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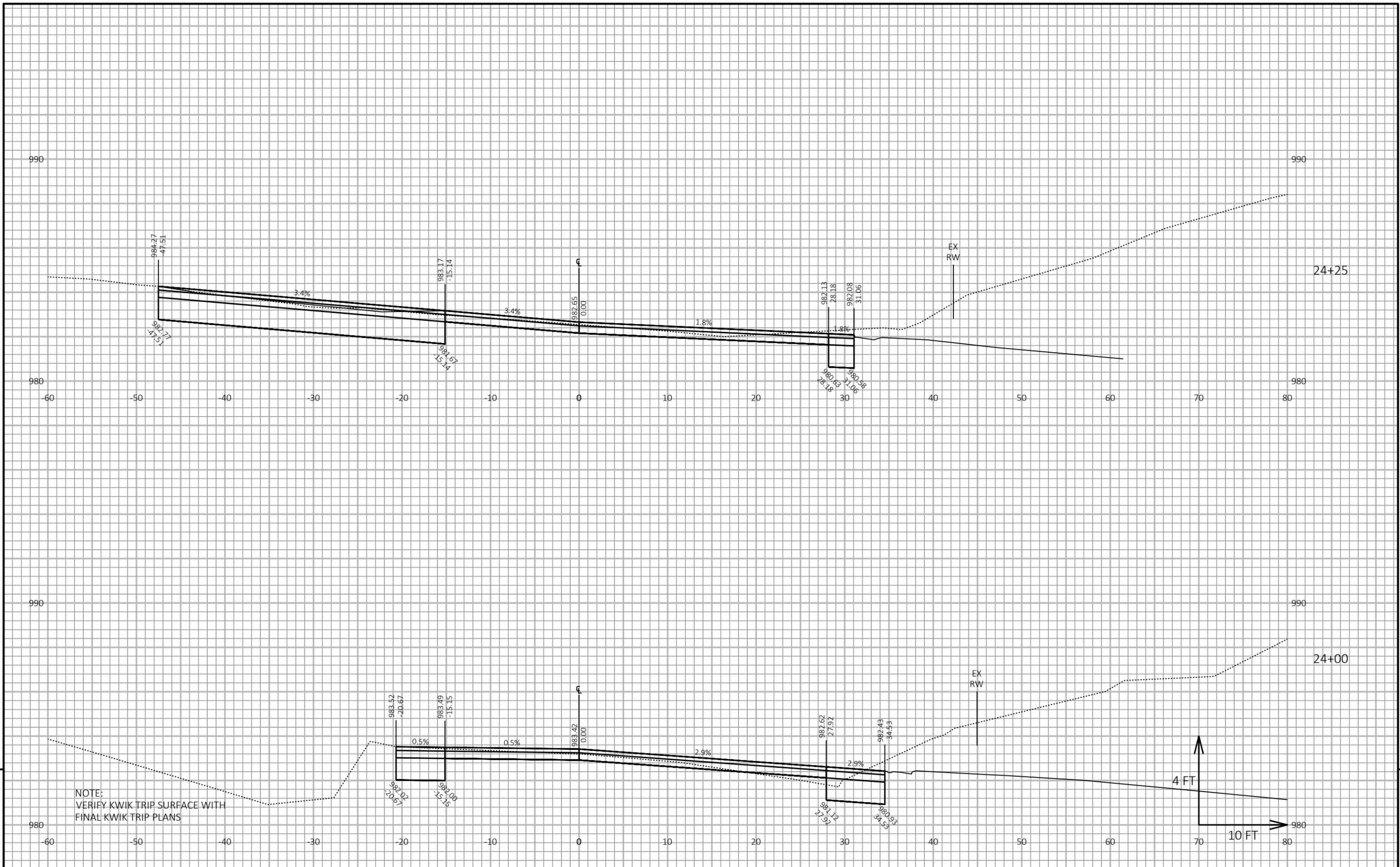
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NOTE:
VERIFY KWIK TRIP SURFACE WITH
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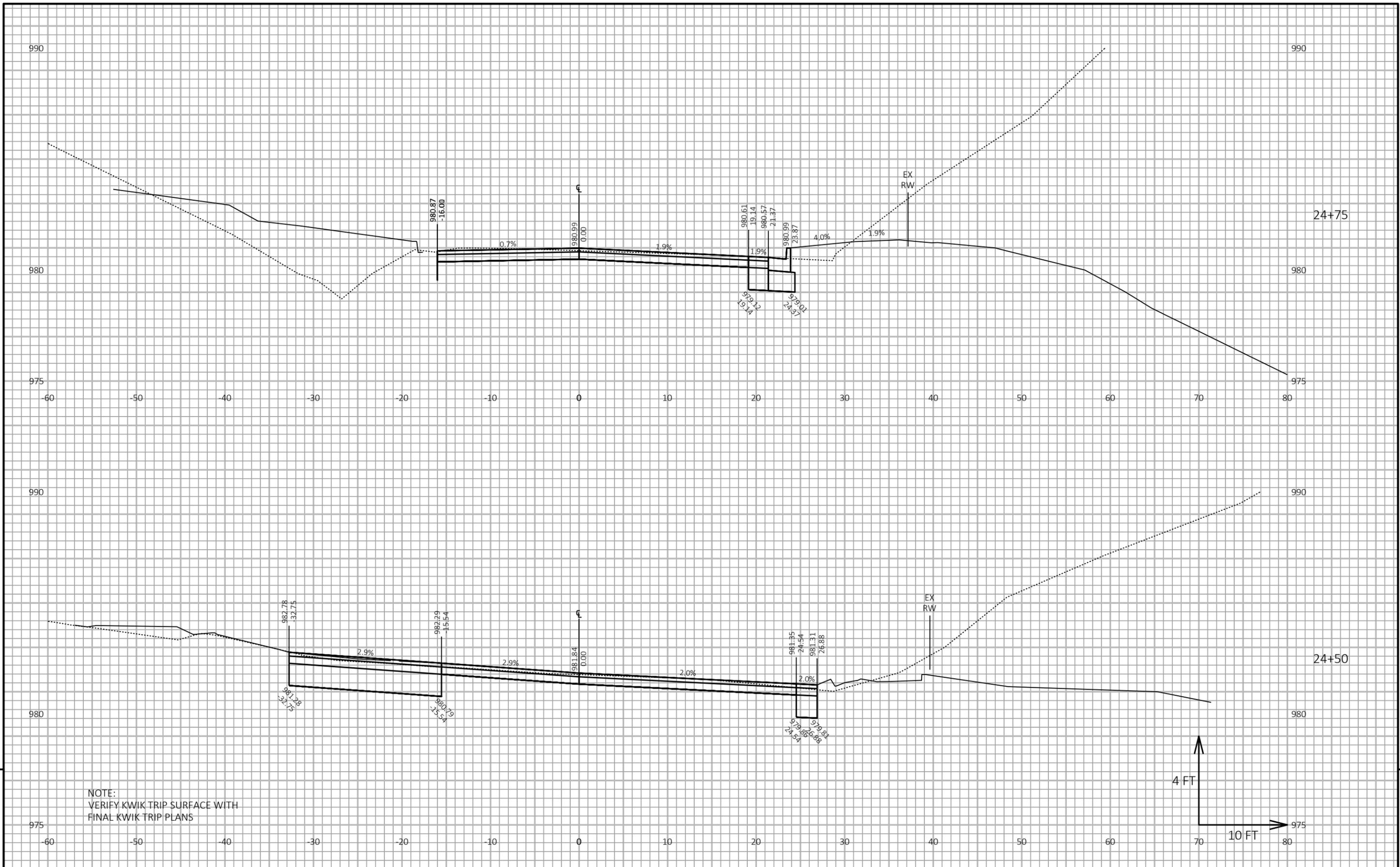
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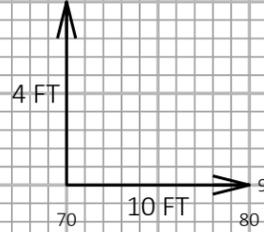
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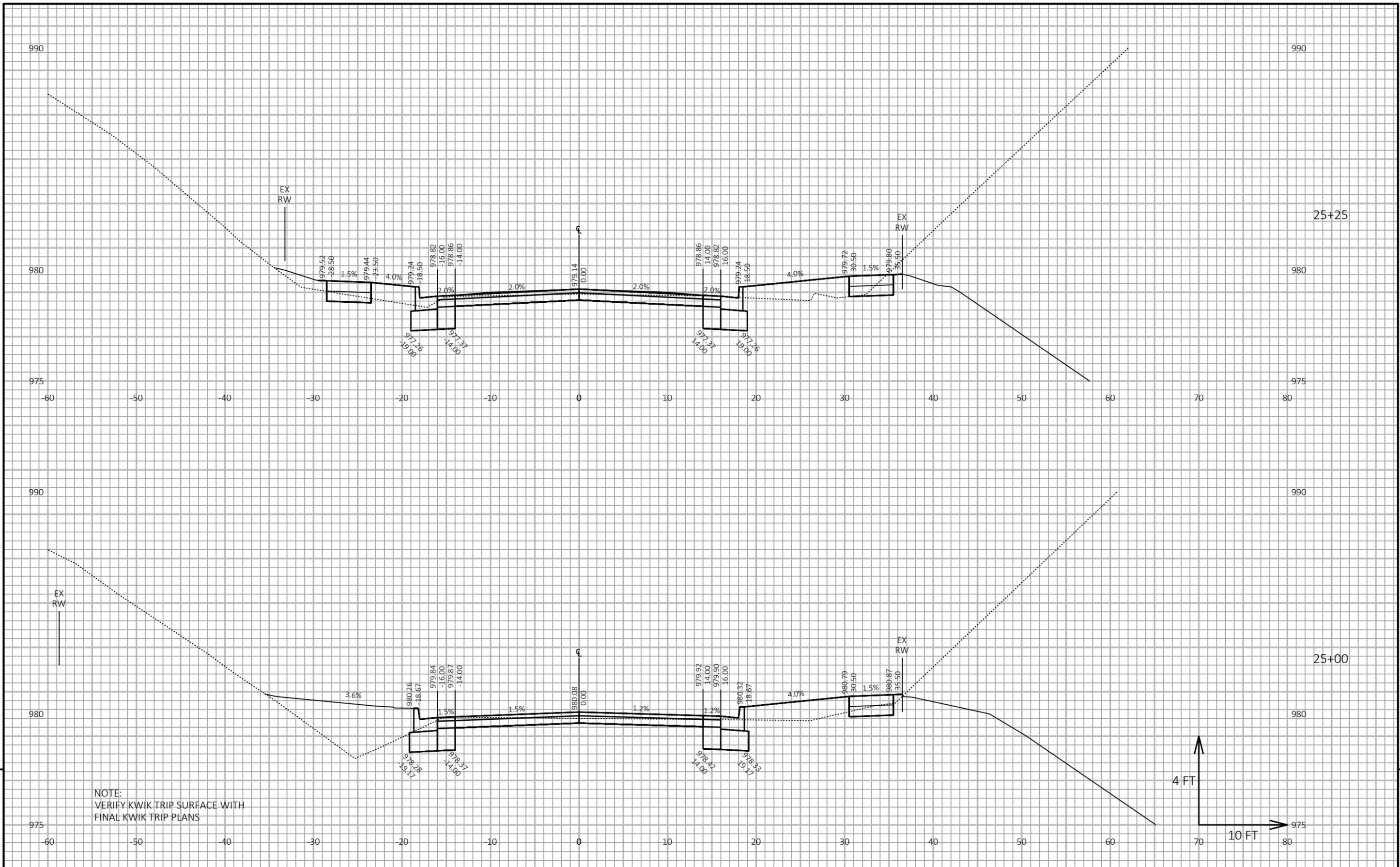
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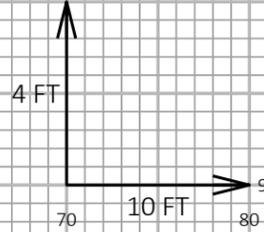


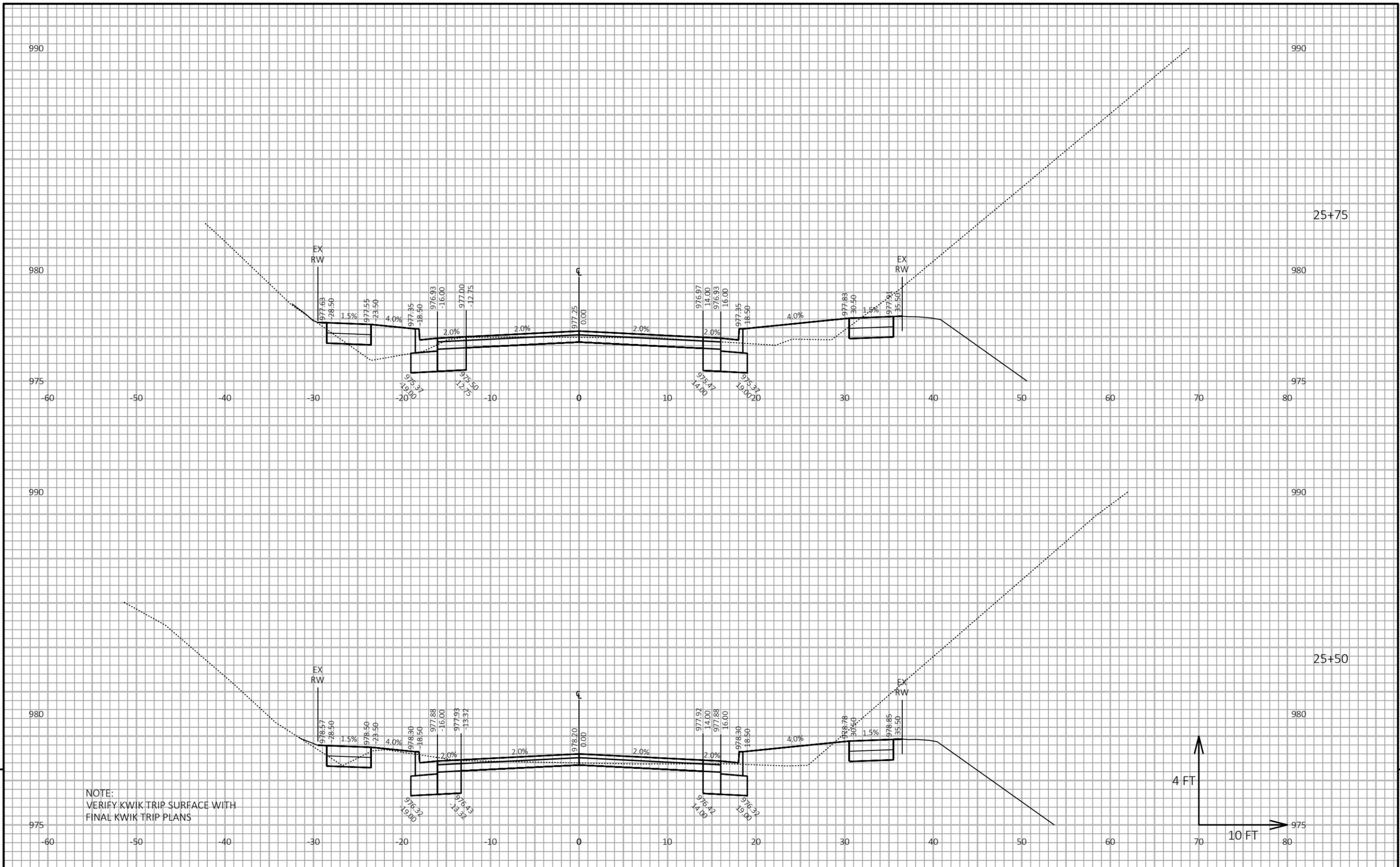
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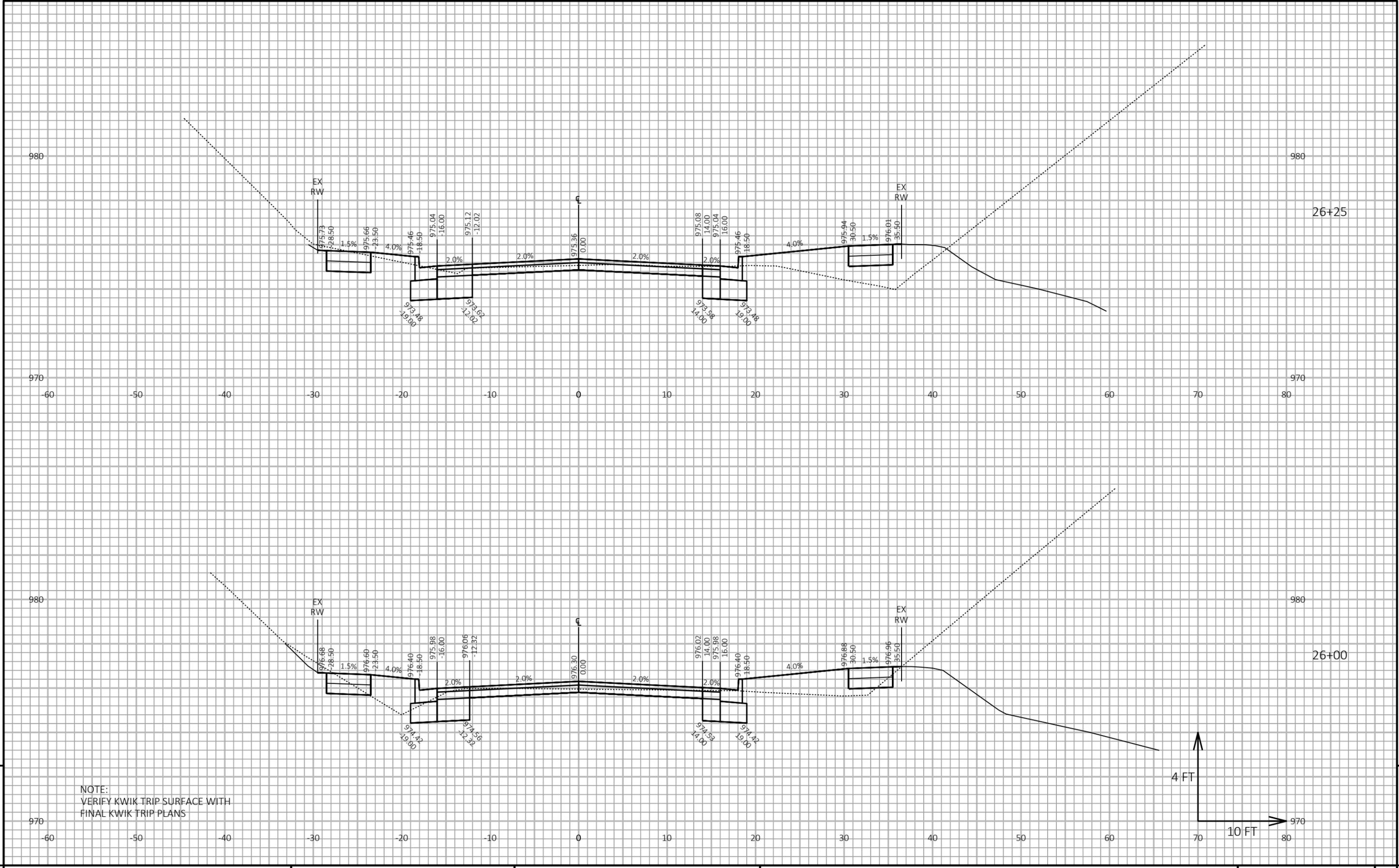
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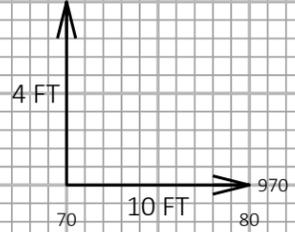
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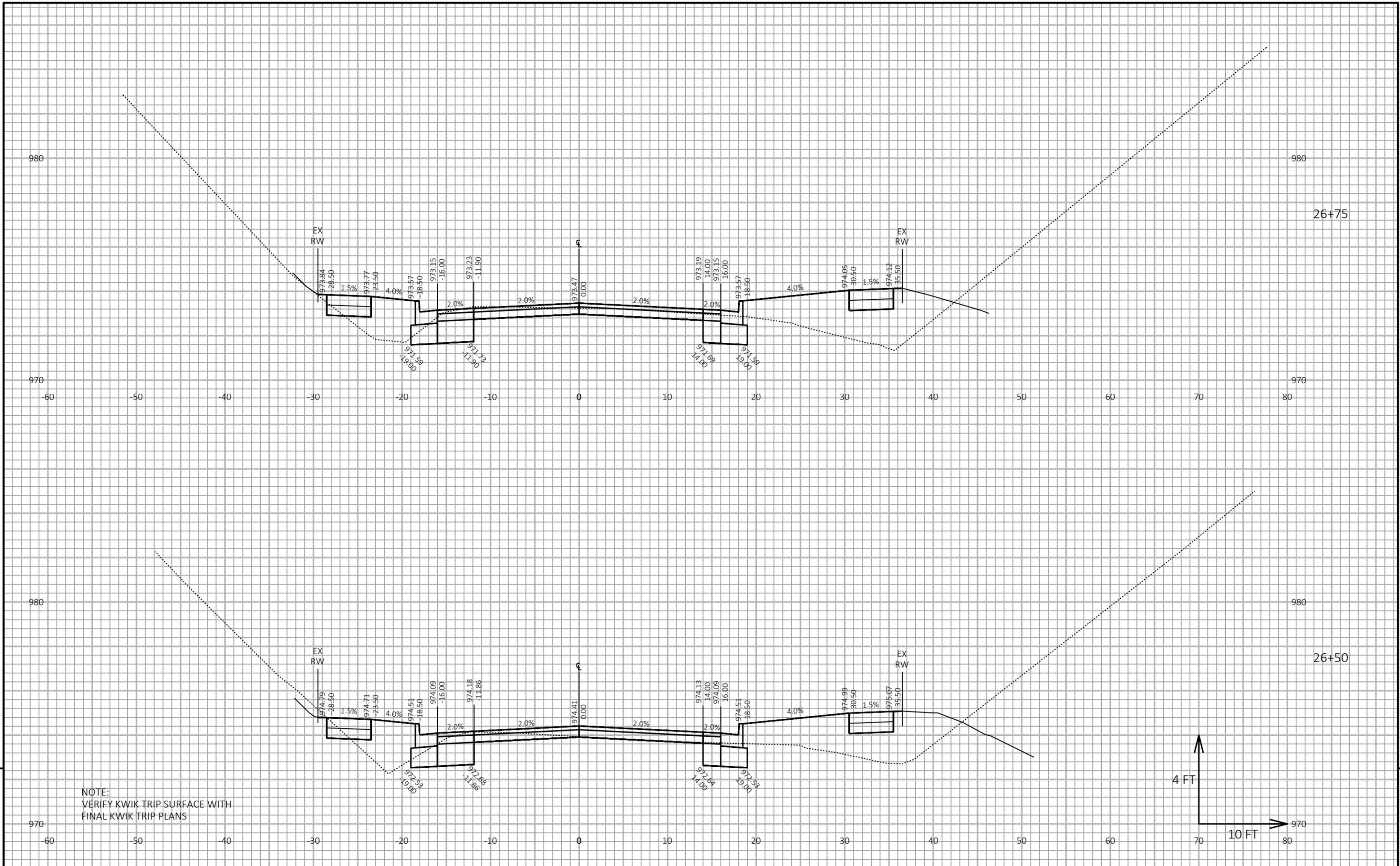


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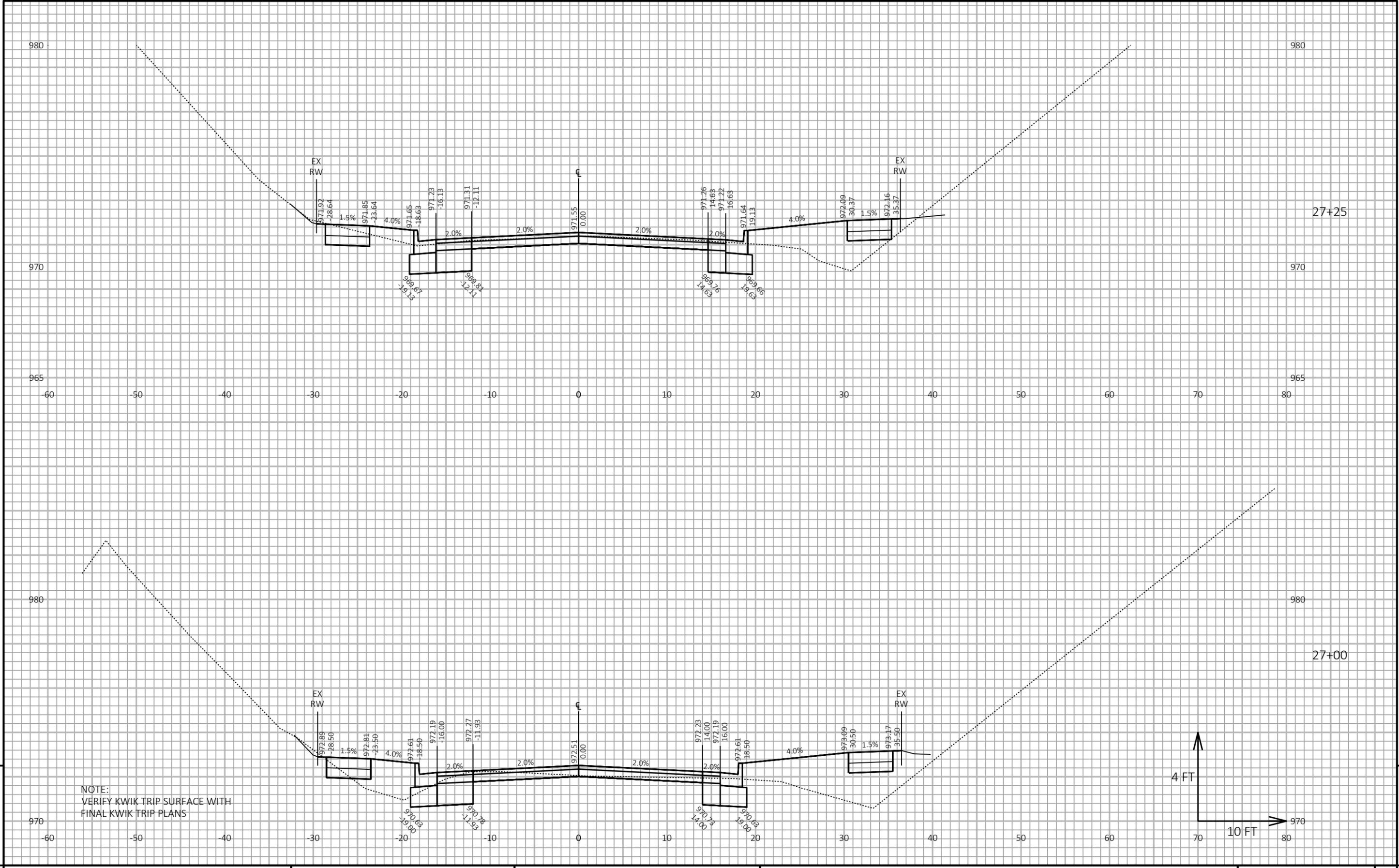
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COUNTY: WAUKESHA

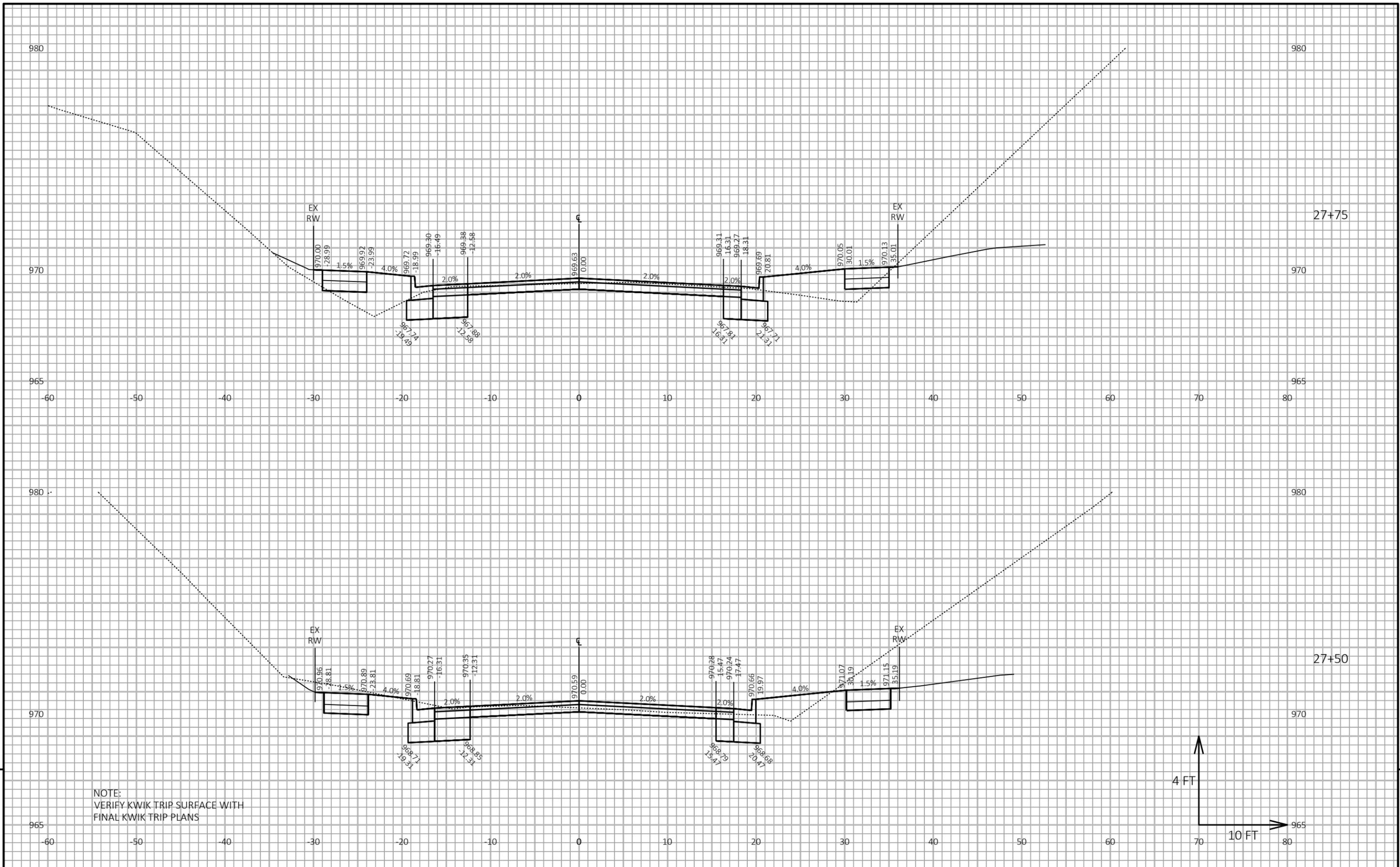
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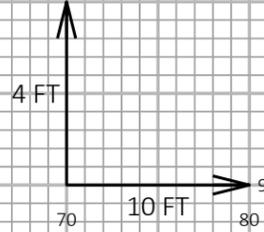
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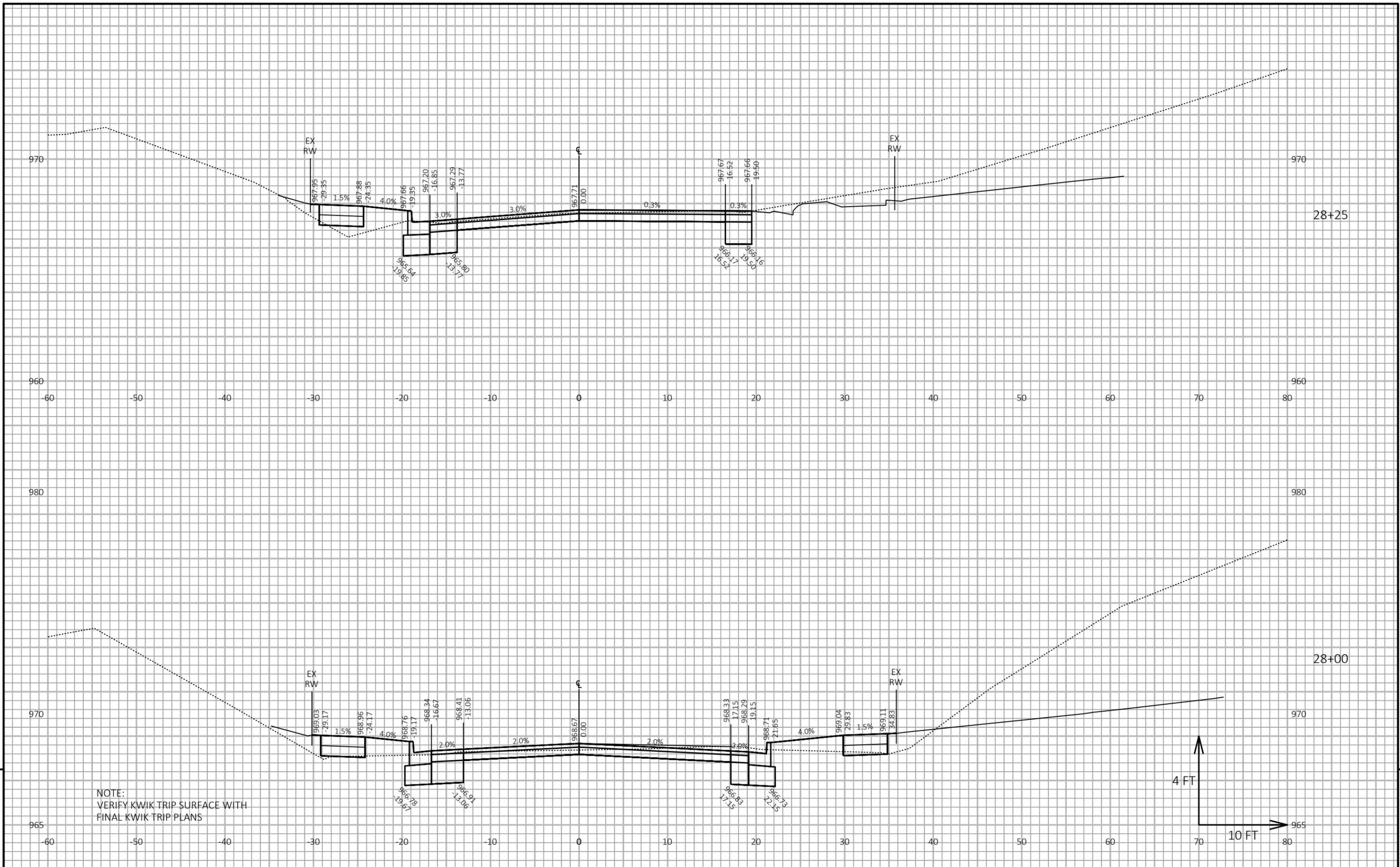


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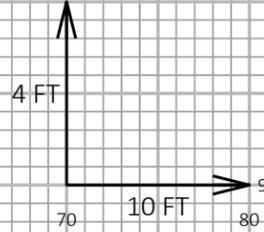


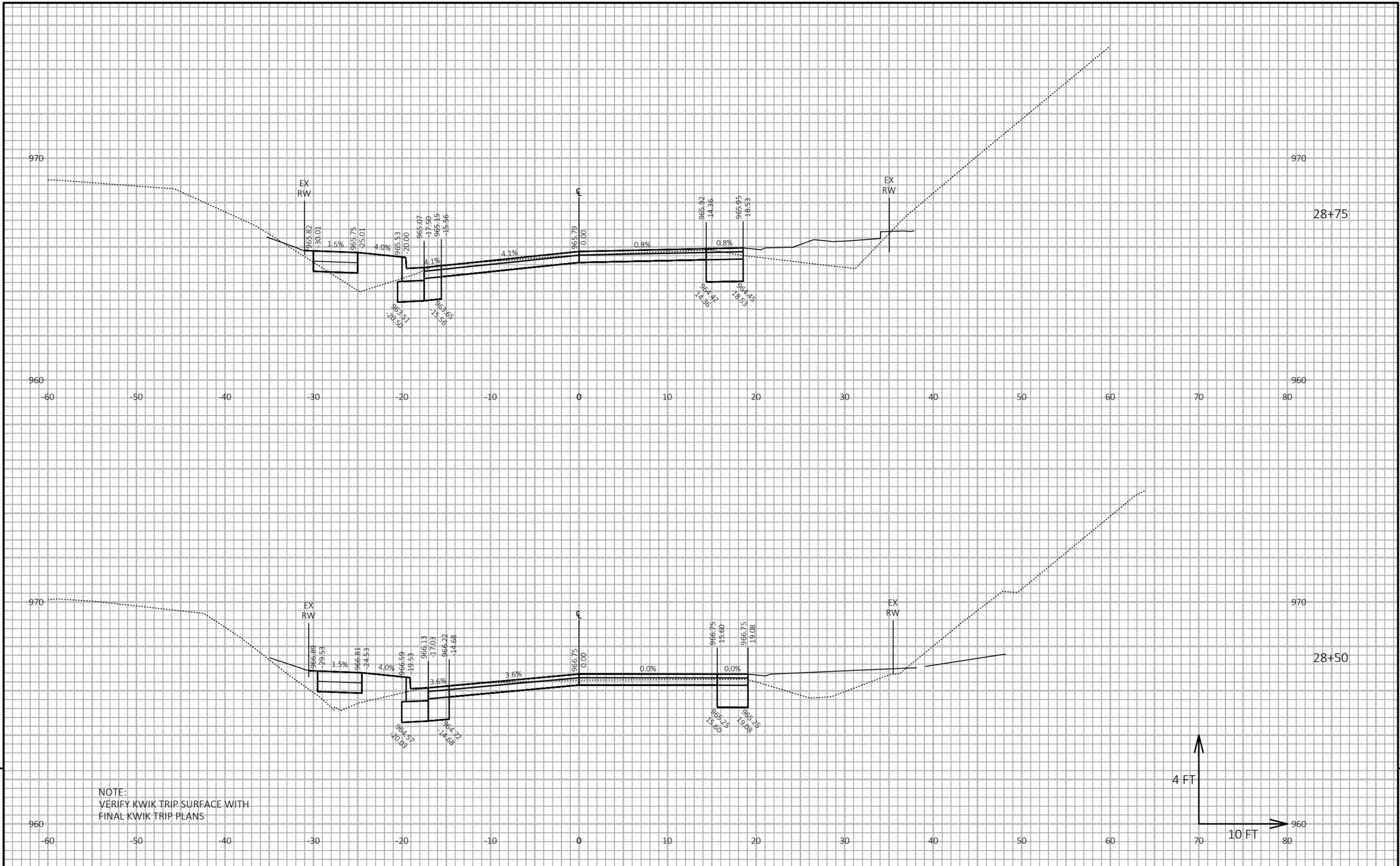
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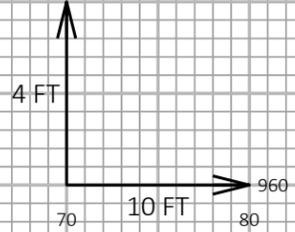




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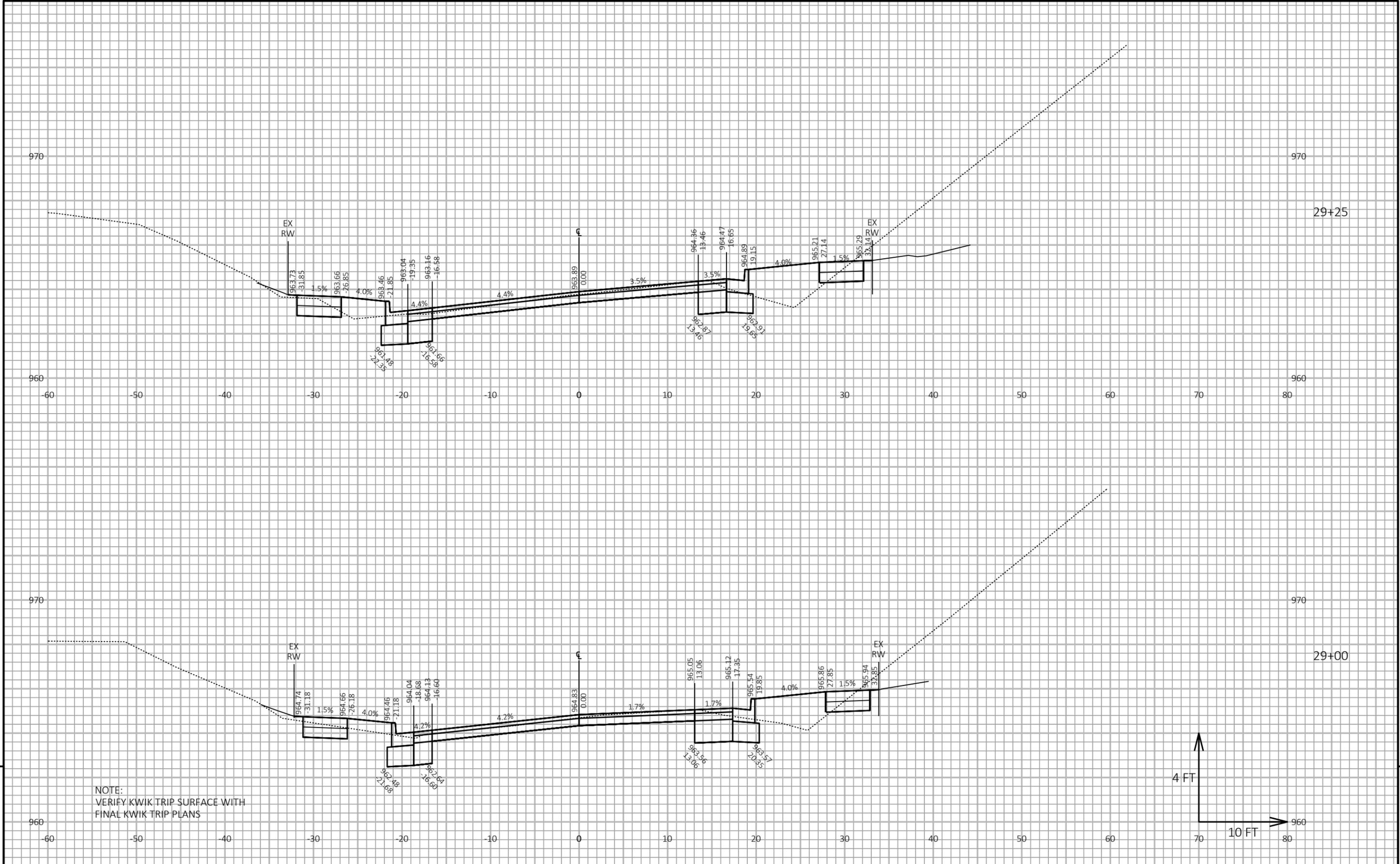
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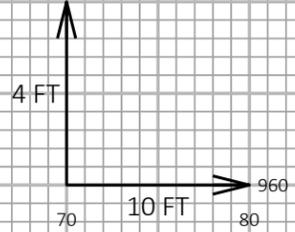


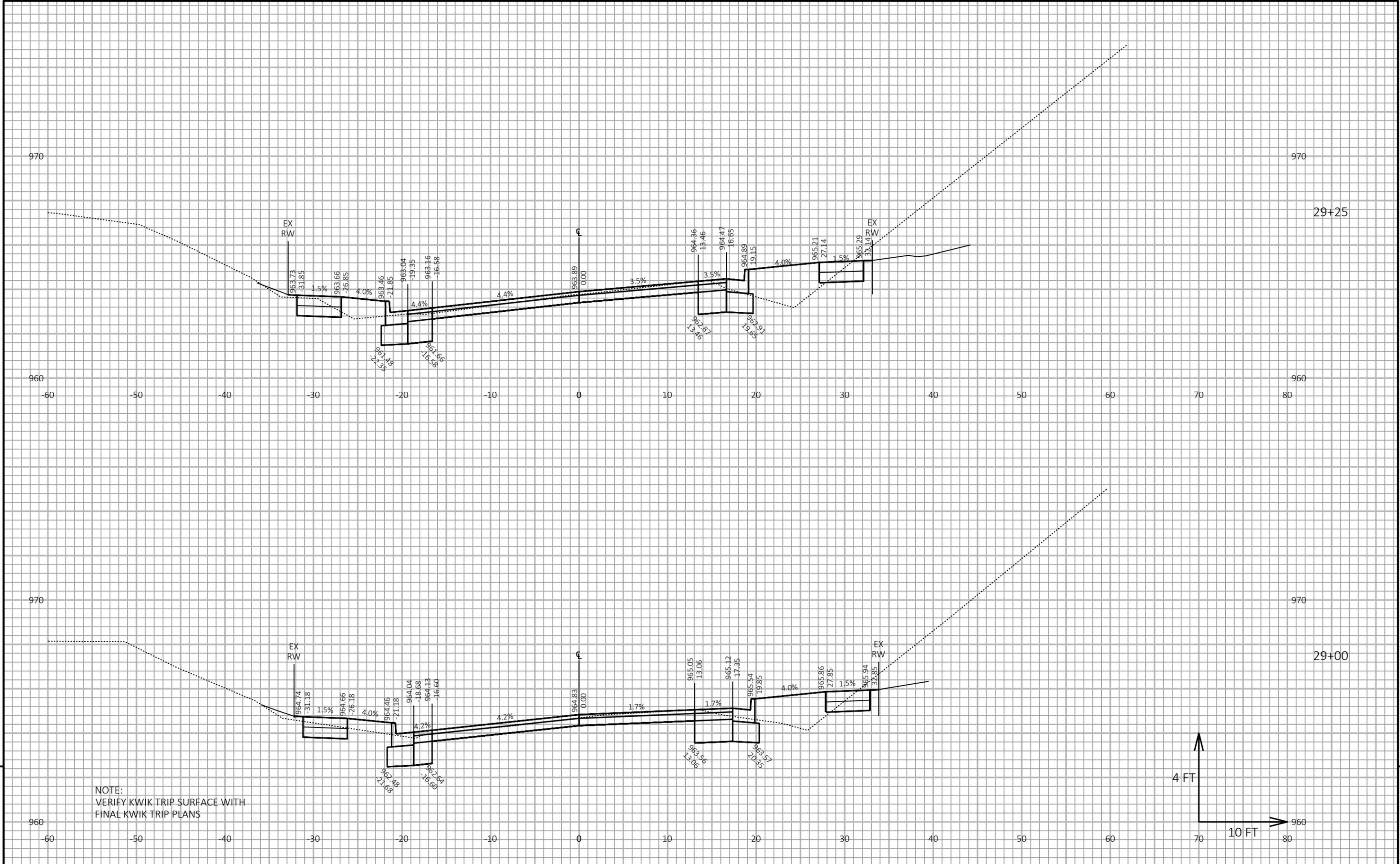
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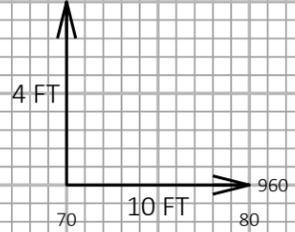


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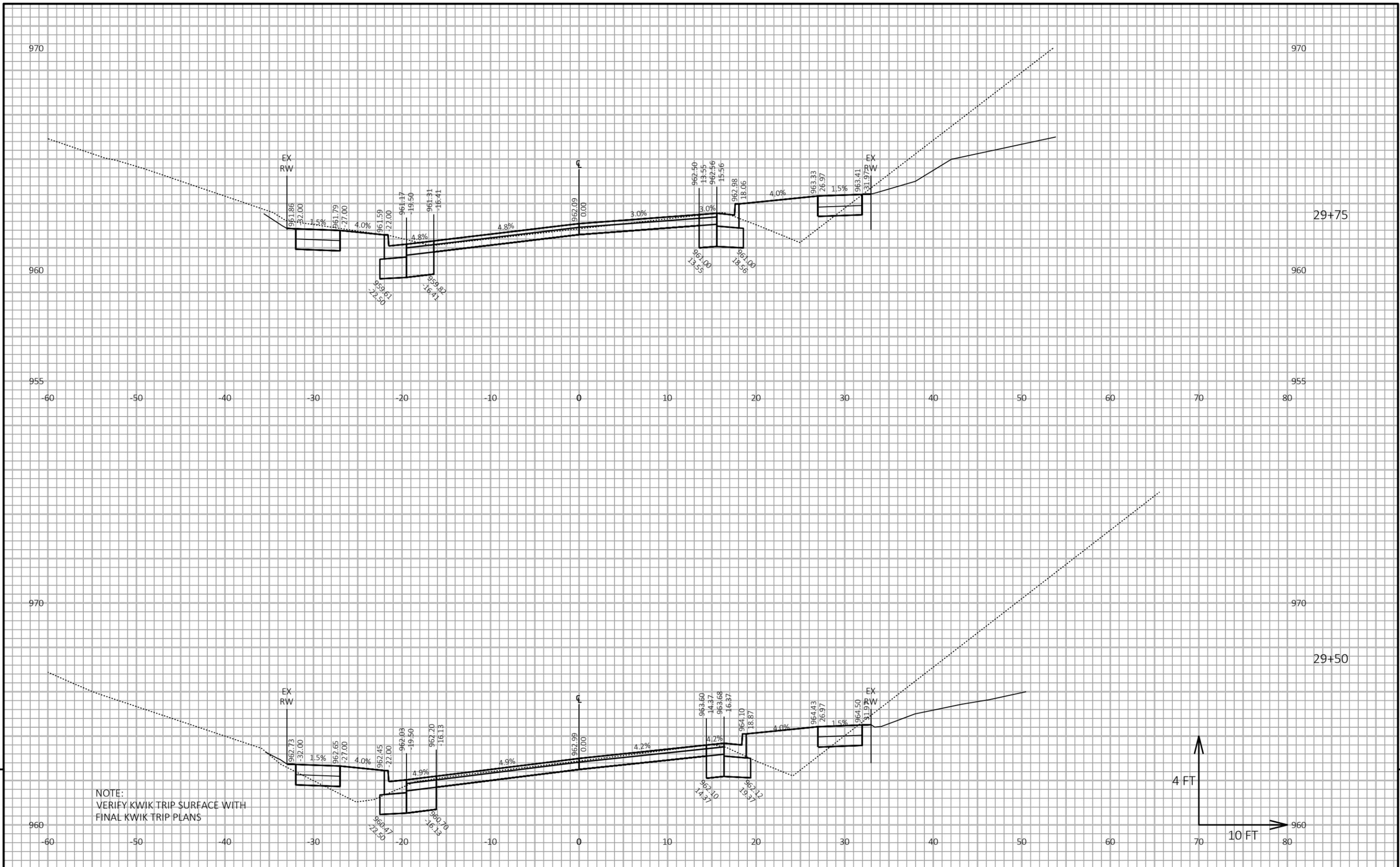




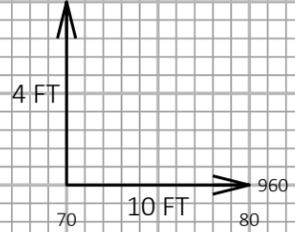
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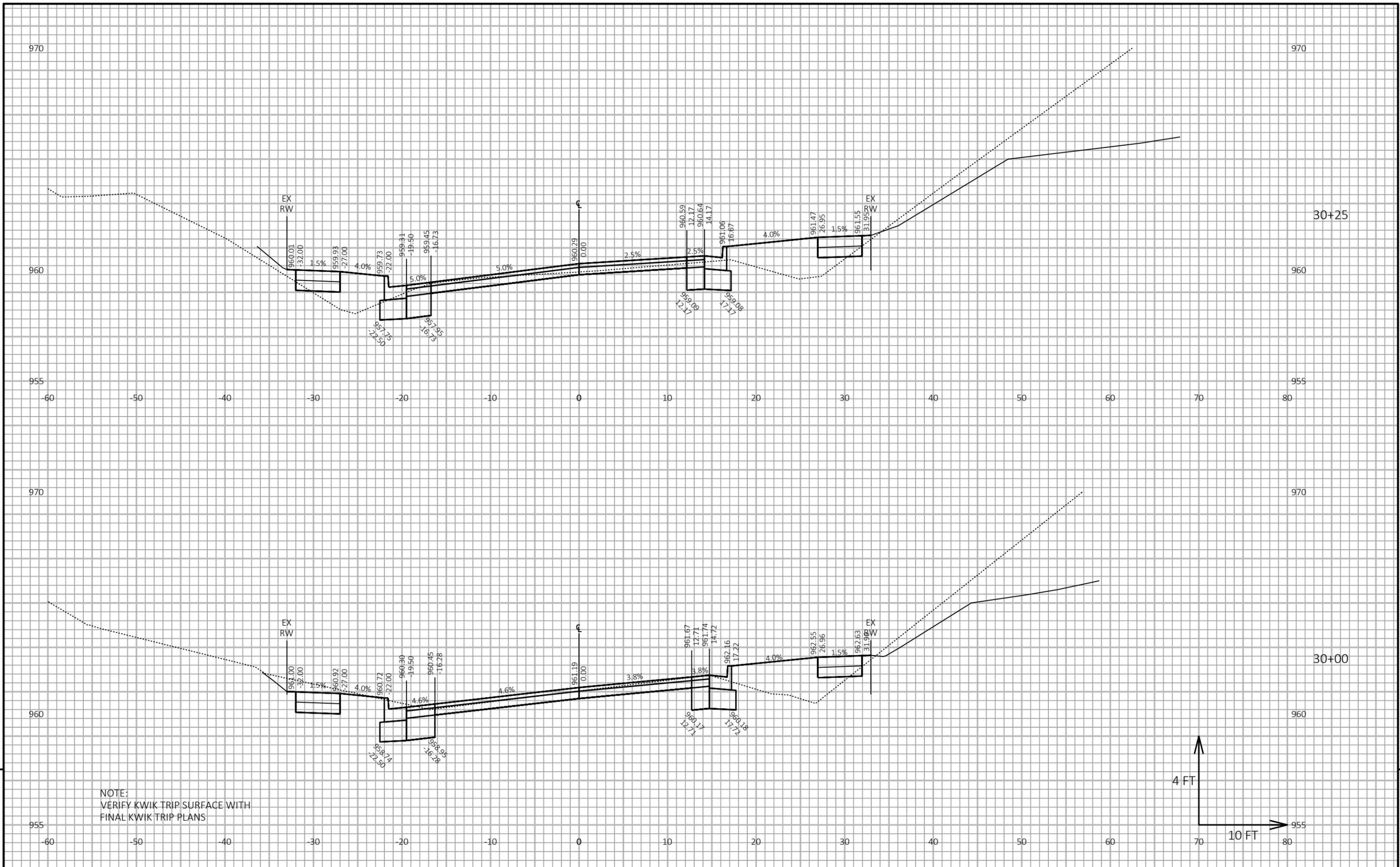


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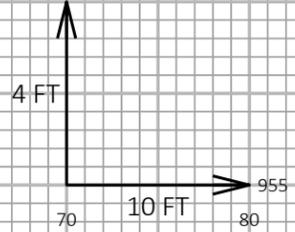


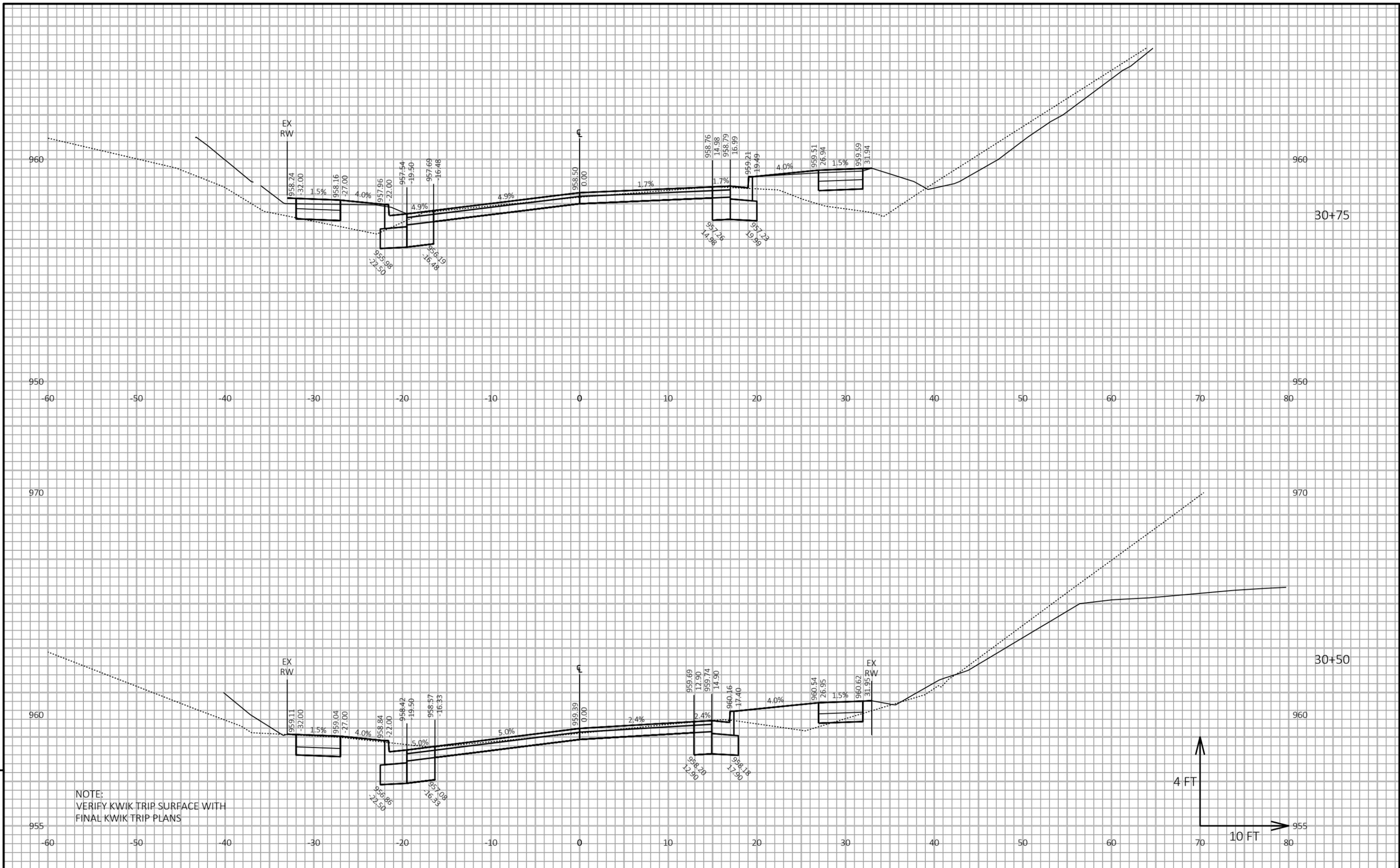
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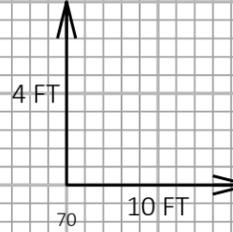


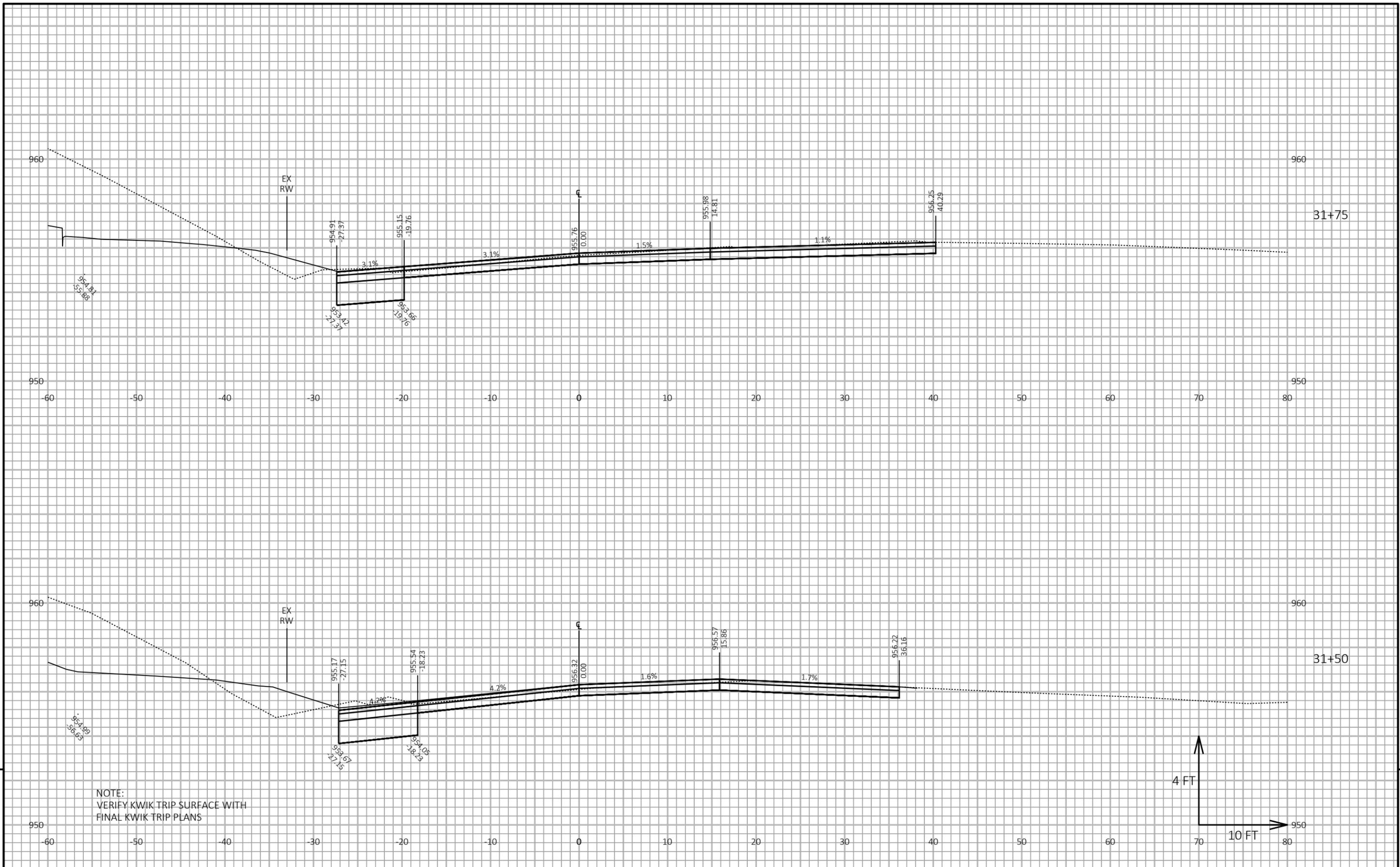
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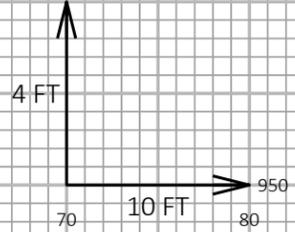


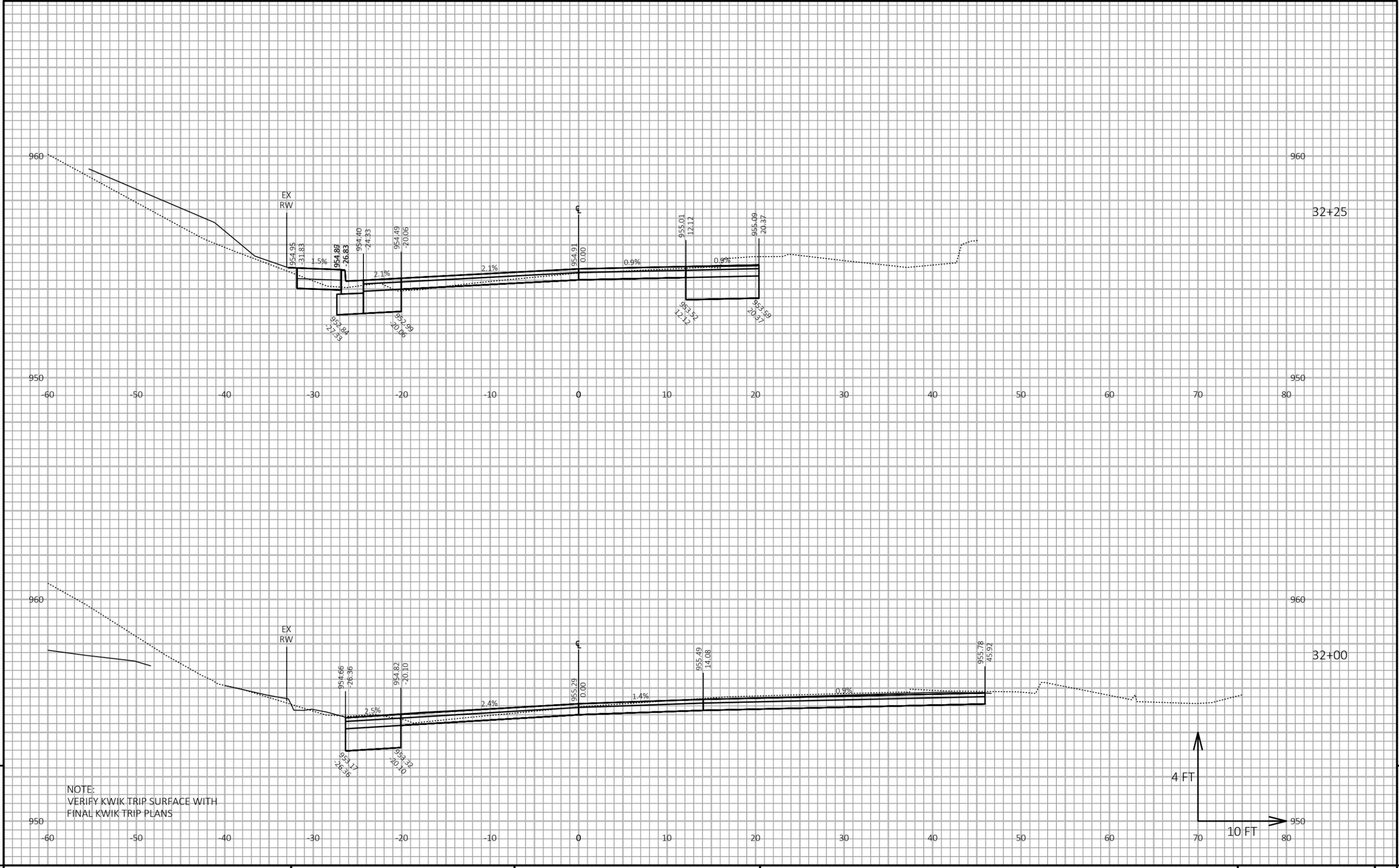
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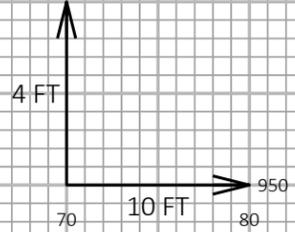


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VERIFY KWIK TRIP SURFACE WITH
FINAL KWIK TRIP PLANS



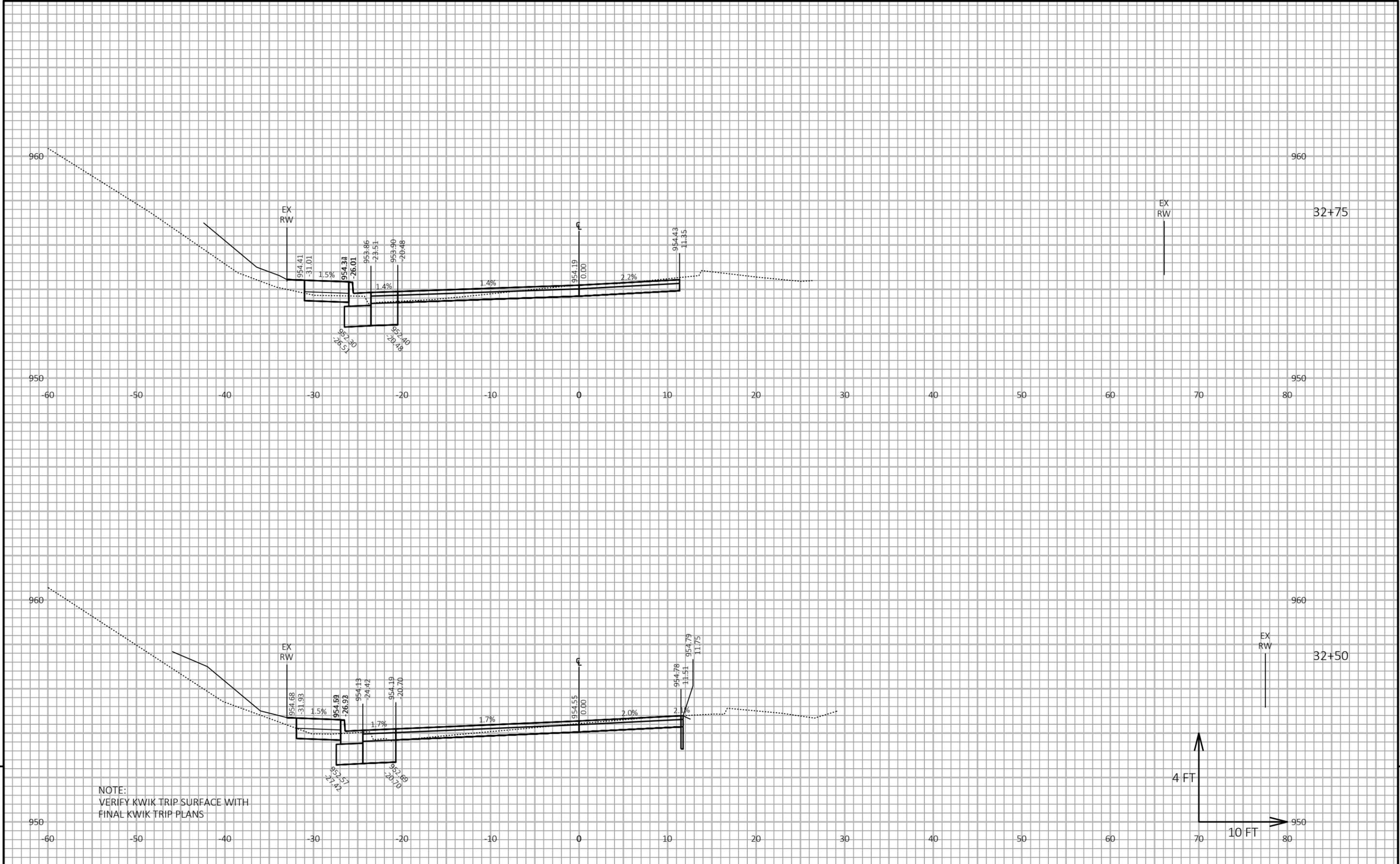


NOTE:
VERIFY KWIK TRIP SURFACE WITH
FINAL KWIK TRIP PLANS

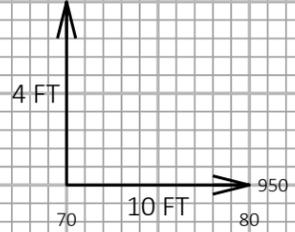


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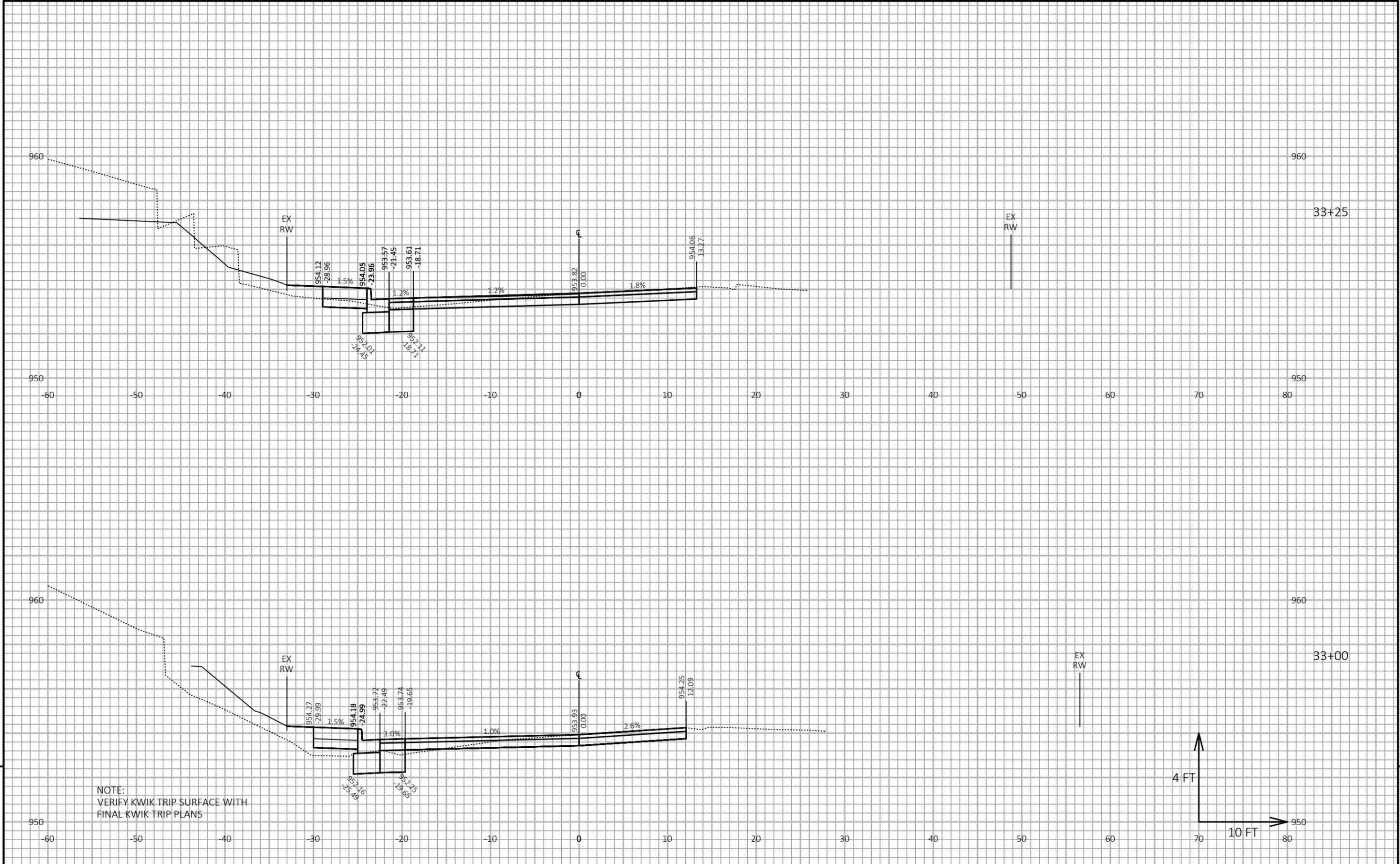
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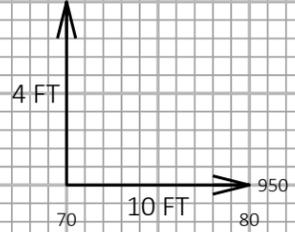
NOTE:
VERIFY KWIK TRIP SURFACE WITH
FINAL KWIK TRIP PLANS



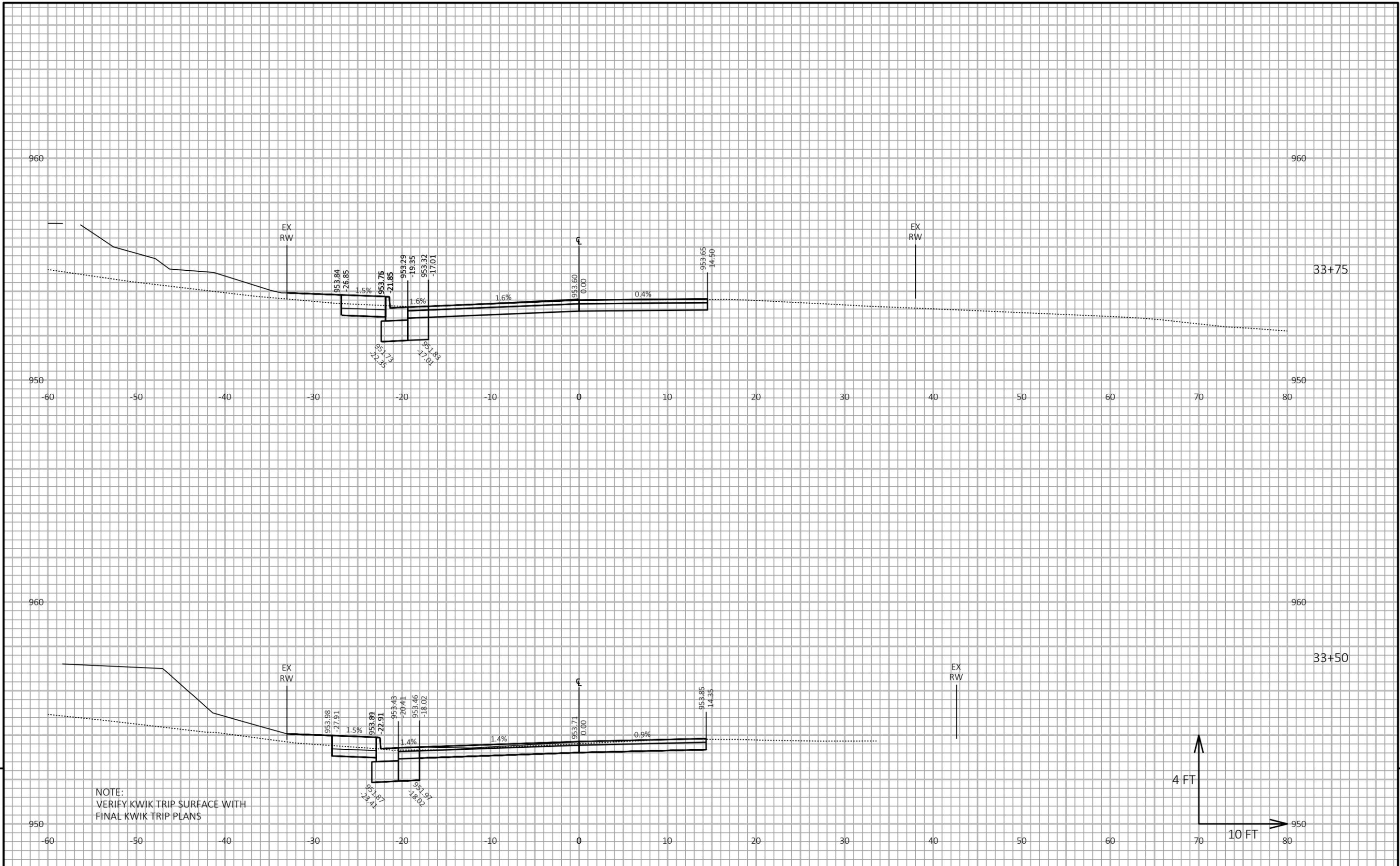
PROJECT NO: 490823	HWY: -	COUNTY: WAUKESHA	CROSS SECTIONS: XS22	SHEET 59	E
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NOTE:
VERIFY KWIK TRIP SURFACE WITH
FINAL KWIK TRIP PLANS



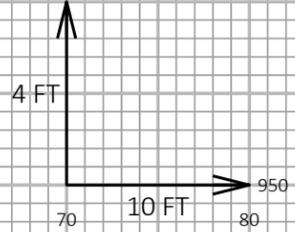
PROJECT NO: 490823	HWY: -	COUNTY: WAUKESHA	CROSS SECTIONS: XS23	SHEET 60	E
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33+75

33+50

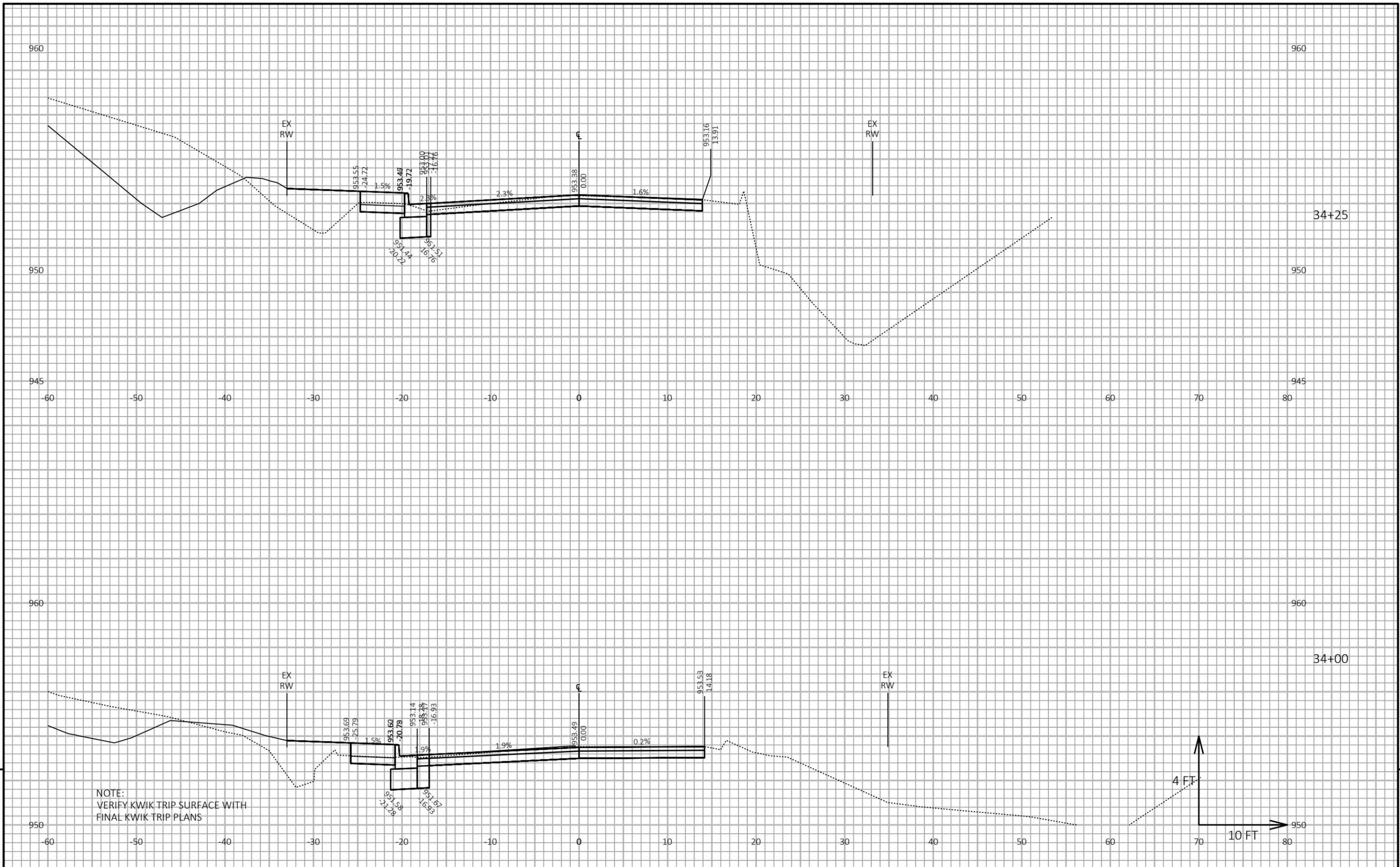
NOTE:
VERIFY KWIK TRIP SURFACE WITH
FINAL KWIK TRIP PLANS



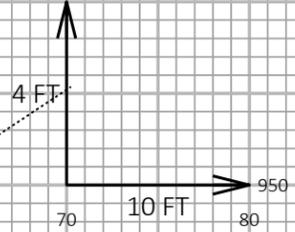
9

9

PROJECT NO: 490823	HWY: -	COUNTY: WAUKESHA	CROSS SECTIONS: XS24	SHEET 61	E
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NOTE:
VERIFY KWIK TRIP SURFACE WITH
FINAL KWIK TRIP PLANS



9

9

PROJECT NO: 490823	HWY: -	COUNTY: WAUKESHA	CROSS SECTIONS: XS25	SHEET 62	E
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